

Park County North Powell Bicycle and Pedestrian Facilities Plan

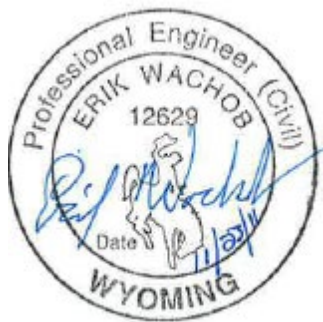
Final Report

November 2021



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EA Job No. 20038

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Introduction

1. Statement of Problem

Pedestrians and bicyclists in and around Powell, Wyoming, within Park County, have limited access to dedicated pathways, bike lanes, or other facilities that provide separation from vehicle traffic. Several public buildings and campuses serving the area are located on the north side of the City, along with numerous parks and residential neighborhoods. Citizens and county officials are particularly concerned about traffic and pedestrian conflicts along Lane 8 and Road 10, roads that see frequent truck use and are signed as truck routes.

The purpose of this study is to identify opportunities for improving pedestrian safety, increase access to pathways in and around the northern portion of Powell, and present conceptual cost estimates for those alternatives.

2. Scope of Study

The study was conducted with several main components. First, a scoping meeting was held with numerous stakeholders, with input solicited from government, public, and private entities. Secondly, EA reviewed existing plans and documentation from the County, City of Powell, and WYDOT. Next, EA performed field reconnaissance of the area to identify existing and potential improvements, design constraints, and considerations. To obtain additional public input, an online survey was prepared and widely advertised. The community received the survey well, with larger participation than would be seen from a public meeting. Survey results were summarized for decision-makers. Finally, individual alternatives were formulated, and schematic cost estimates were prepared. Recommendations are presented within this report for further consideration, design, and implementation.

3. Overview of Study Area

The study area (shown in Exhibit 1) encompasses an area both within and outside of the City limits, generally as follows, and including the streets referenced:

- North of Seventh St;
- West of Road 8, known as Panther Blvd within City limits;
- South of Lane 8;
- East of Lane 10, known as Tower Blvd within City limits.

Within City limits, the area is typically residential, with several parks, schools, and other public facilities. The area is seeing growth with several new subdivisions platted or in process. Outside City limits, the ground is generally used for irrigated agriculture, with productive, well-drained soils and level topography.

Important public facilities within or adjacent to the study area include the following:

- Park County Annex
- Northwest College
- Powell High School
- Homesteader Park
- Park County Fairgrounds
- Other public facilities including several churches, day-cares, and preschools.

NO PEDESTRIAN FACILITIES
LANE 8 OR ROAD 10

NO PEDESTRIAN FACILITIES
LANE 8 OR ROAD 8 THIS
VICINITY

NO SIDEWALK ON NORTH
SIDE OF 7TH ST

POWELL CITY
LIMITS

PARK CO. ANNEX

WYO HWY 295

PATHWAY
CROSSES
LANE 8

EXISTING GRAVEL
PATHWAY
"LATERAL C"

EXISTING
PAVED
PATHWAY

NORTHWEST
COLLEGE

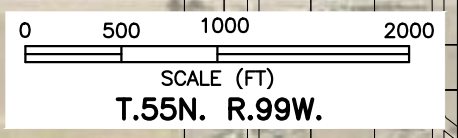
HIGH SCHOOL

PARK
COUNTY
FAIRGROUNDS

HOMESTEADER
PARK

CITY OF
POWELL

US 14A (COULTER AVENUE)



P:\2020\20038.00 - Park County Walking And Biking Study\AutoCad\20038_Base.dwg STUDY AREA 9/28/21 ERIK

DATE	DRAWING LOG	BY	CHECKED	APPROVED
09/28/21	FINAL REPORT	LPU	EDW	TLC

DRAWN BY: LPU
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**ENGINEERING ASSOCIATES
 CONSULTING ENGINEERS & SURVEYORS**

OWNER:
PARK COUNTY

PROJECT: **NORTH POWELL PEDESTRIAN AND BICYCLE FACILITIES**
 TITLE: **STUDY AREA**

EXHIBIT 1

4. Funding of this Study

This report is sponsored by Park County, with funding from the Wyoming Department of Transportation's (WYDOT) Transportation Alternatives Program (TAP) and Park County.

5. Stakeholders, Agencies, and Meetings

In addition to Park County, comments were solicited from the following entities:

- WYDOT
- Park County annex building staff
- Park County Fairgrounds
- City of Powell
- Northwest College
- Park County School District #1
- The public at large.

The scoping meeting was held on September 9, 2020, and included the stakeholders above. The agenda and sign-in sheet are found in Appendix E.

A second meeting was held in October 2020 to solicit input from the City of Powell staff, including the city administrator, and representatives from Public Works, the Streets Department, and the Parks Department.

A final formal meeting was held in November 2021 to solicit input from stakeholders after circulation of a draft of this report. The results of that meeting are summarized in the "Stakeholder Input" section near the end of this report.

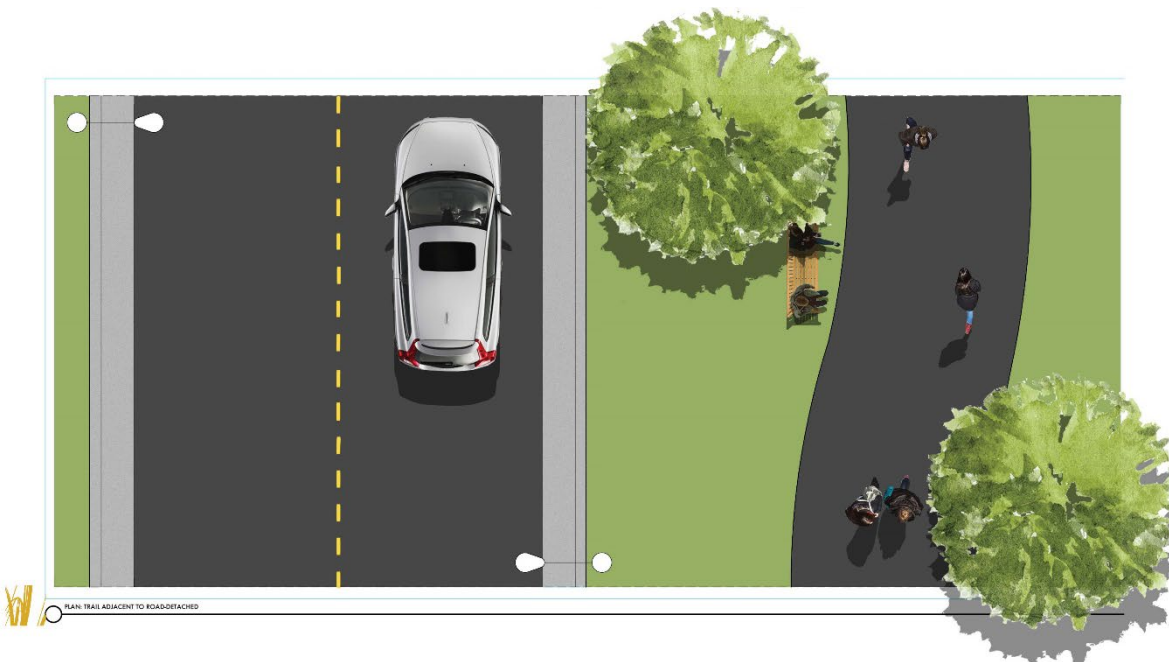


Figure 1 - Detached winding pathway along roadway (Tallgrass Landscape Arch.)

Previous Studies and Plans

1. Powell Streets Master Plan

The current Master Street Plan for the City is from 2013, prepared by DOWL HKM. That document addresses transportation throughout the City. It includes sections relating to the bicycle and pedestrian facilities; and to traffic on the arterials around the north side of the City (Roads 8 and 10, Lane 8). The Master Plan was adopted by the City Council for use in guiding development as relates to transportation.

The plan identifies locations for proposed new pathways, which is included here as Appendix D. Roads 8 and 10, Lane 8, and several other connectors are proposed locations for 12-ft wide pathways. Seventh Street, Division Street, and locations around the high school are proposed for 6-ft wide pathways. It should be noted that many of the pathway routes considered as alternatives later in this report are consistent with the locations shown in the 2013 Master Plan.

The plan also identifies preferred truck routes around town (see section 7.0 of the 2013 Master Plan). It states:

“Lane 8, Lane 10, Road 8 and Road 10 are ideal corridors for future arterial roads,” and they “should be developed to accommodate truck traffic and designated as truck routes.”

To accomplish this, the report further notes:

“Acquiring adequate right-of-way along these corridors, planning for truck turning at intersections, accommodating irrigation ditches and facilities, and enforcing access management during their development is critical to provide an efficient transportation network.”

To accommodate bicycle and pedestrian access along with truck and light vehicle traffic on those routes will require planning for non-motorized uses at the onset of any road improvement project undertaken on those county routes.

A previous (2011) Safe Routes to School Report was prepared for the City by Inberg Miller Engineers, but was largely superseded by the 2013 Master Plan with respect to pathways.

Community Survey

1. Methods and Participation

Community feedback was solicited through an online survey. The online survey format was selected in lieu of a public meeting, workshop, or similar due to COVID-19. With about 250 respondents, the number of participants was much higher than typically have attended similar community meetings in the area.

The survey consisted of six questions, five of which were multiple choice. The survey intended to inform how often respondents used pathways; where they typically started and stopped their journeys; how they used existing pathways and roadways; and what their concerns were. The final question allowed for respondents to offer their thoughts on pathways and safety in and around Powell.

The survey was featured in a front-page article by the Powell Tribune, February 4, 2021. Participation in the survey jumped significantly in the days following publication.

**PARK COUNTY
NORTH POWELL PATHWAY SURVEY**

**NEW POWELL BIKE PATH?
DO YOU USE BIKE PATHS FOR:**

- BIKING
- RUNNING
- WALKING
- EXERCISING DOGS
- GETTING IN SHAPE?

TELL US WHAT YOU THINK.

SCAN THE QR CODE TO TAKE OUR QUICK 6-QUESTION SURVEY

DON'T HAVE THE QR READER? VISIT: [HTTPS://WWW.SURVEYMONKEY.COM/R/PGNFQYW](https://www.surveymonkey.com/r/PGNFQYW)

Park County is conducting a planning study of pathways in the north Powell area as part of a federally-funded Transportation Alternatives Program project administered by Wyoming Department of Transportation. The goal of the study is to identify pathway improvements to improve safety while minimizing vehicle vs. pedestrian and vehicle vs. bicycle conflicts. This study is focused on areas north of State Highway 16A and south of Lane 8, between Road 10 (Tower Blvd.) and Road 9 (Panther Blvd). This includes Northwest College, NWC west campus, Powell High School, Fairgrounds, Homebreaker Park, Park County Annex, and related areas. Transportation Alternatives are federally funded, community-based projects that expand travel choices and enhance the transportation experience by integrating modes and improving the cultural, historic, and environmental aspects of our transportation infrastructure.

www.parkcounty.us/publicworks/publicworks.html

SCAN THE QR CODE

Figure 2 - Sign advertising survey, placed at multiple locations around study area

2. Survey Questions

The survey asked six questions, shown here:

1. When exercising outdoors or commuting, how often to you ride a bike, walk, or run?
 - a. Never or rarely,
 - b. Once a month,
 - c. Once a week,
 - d. Several times a week.

2. In the last year, to which of the following destinations have you or someone in your household walked or ridden on a bike?
 - a. Powell High School
 - b. Powell Middle School
 - c. Northwest College
 - d. Trapper West neighborhood
 - e. Homesteader Park
 - f. Park County Annex
 - g. Park County Fairgrounds
 - h. Downtown Powell
 - i. None of the above
 - j. Other:

3. How often do you use the existing gravel pathway along Lane 8 and/or near the high school?
 - a. Never or rarely,
 - b. Once a month,
 - c. Once a week,
 - d. Several days a week.

4. In which neighborhood(s) do you most frequently start and/or stop your bike, walking, or running journeys?
 - a. An image was provided:

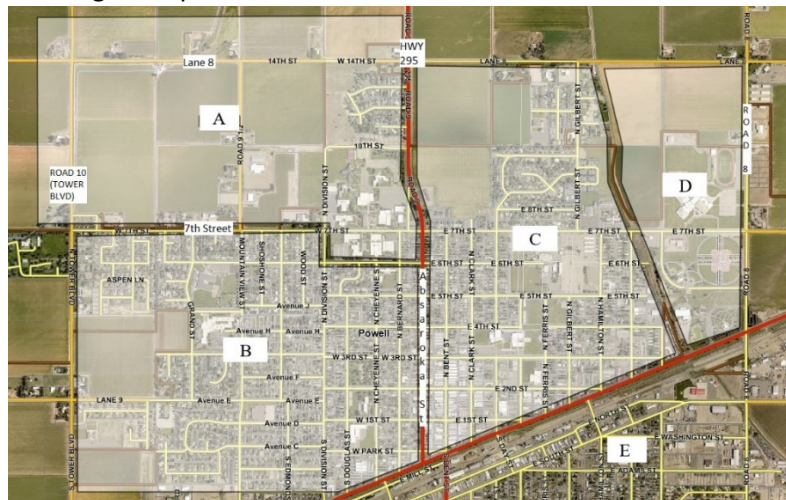


Figure 3 - North Powell neighborhoods for survey purposes

5. When walking, running, or biking, I often follow the following routes (check all that apply):
 - a. Lane 8 east
 - b. Lane 8 west
 - c. Road 10, aka Tower Blvd
 - d. Road 8 north of Homesteader Park
 - e. Absaroka St, aka Hwy 295
 - f. Seventh St west
 - g. Seventh St east
 - h. Road 9 ½
 - i. Other.

6. When walking, running, or riding a bike in the north Powell area, what specific safety concerns do you have? What specific locations do you think are unsafe? Also, please list any other comments, questions, or concerns.

3. Survey Responses

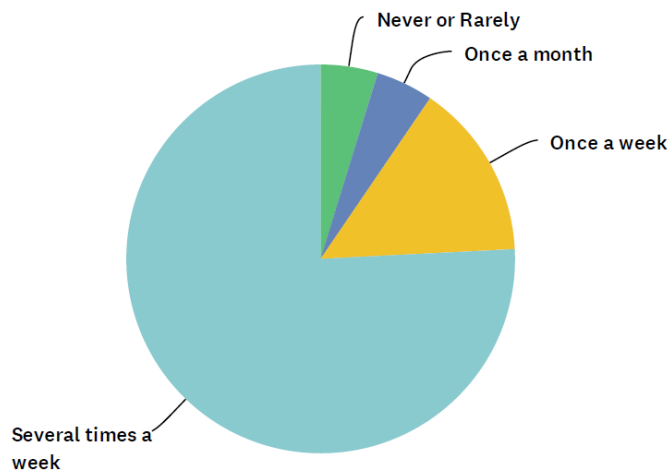
A detailed view of all responses is provided in Appendix B – Community Survey Responses. A summary is provided here. For the complete results from each question, please see the appendix.

a. Frequency of use (Question 1)

Not surprisingly, most respondents said they biked, walked, or ran once a week or more. Less than 10% of respondents do those activities never, rarely, or once a month.

Q1 When exercising outdoors or commuting, how often do you ride a bike, walk or run?

Answered: 252 Skipped: 0

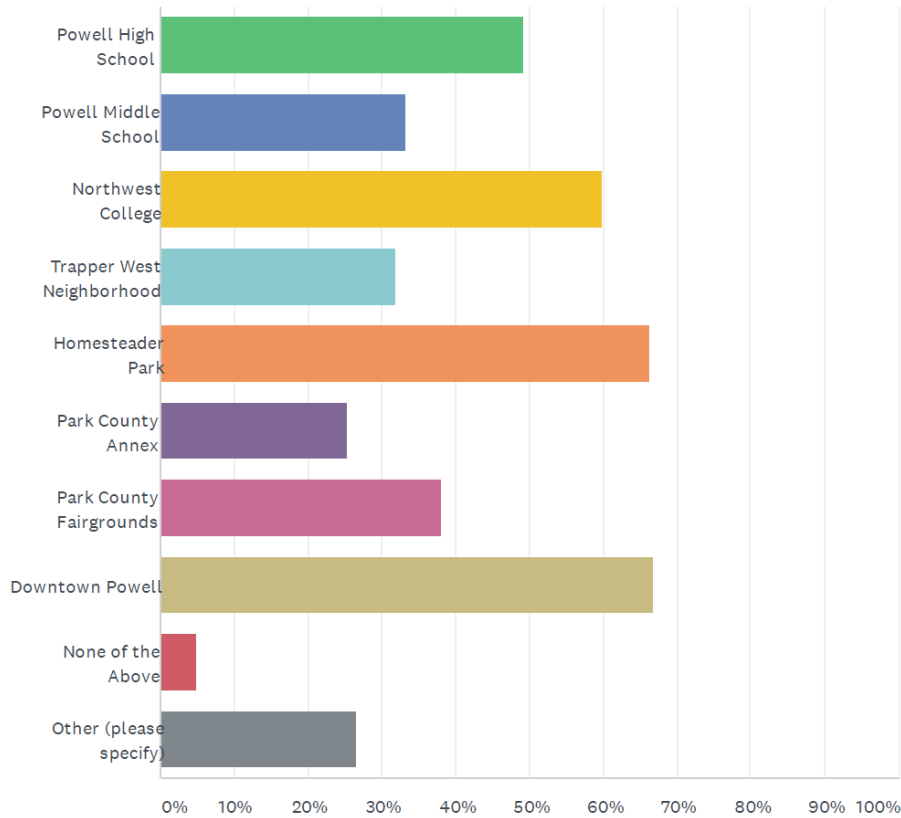


b. Destinations (Question 2)

Respondents commuted by walking or biking most often to NWC, Homesteader Park, and downtown Powell, with more than 50% of respondents visiting those destinations within the last year. Powell High School was at almost 50%.

Q2 In the last year, to which of the following destinations/area have you or someone in your household walked or ridden on a bike?

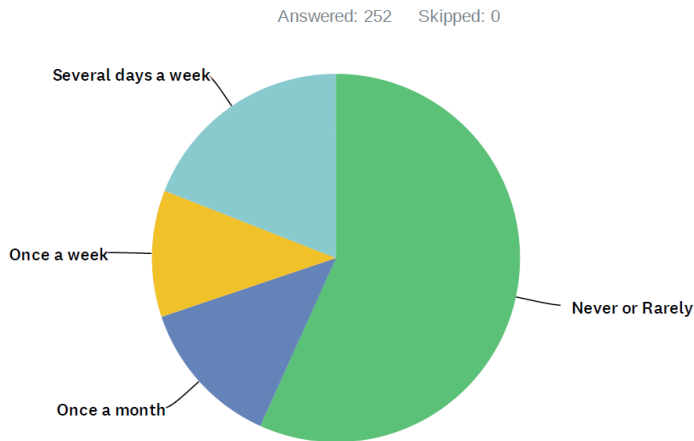
Answered: 252 Skipped: 0



c. Existing gravel pathway (Question 3)

The existing gravel pathway near the High School and along Lane 8 is infrequently used by most survey respondents, with 70% of respondents never, rarely, or only once a month using it. However, 19% of respondents state using it several days per week. It is likely that those respondents nearest the pathway frequently use it, while residents more distant to the pathway use other routes.

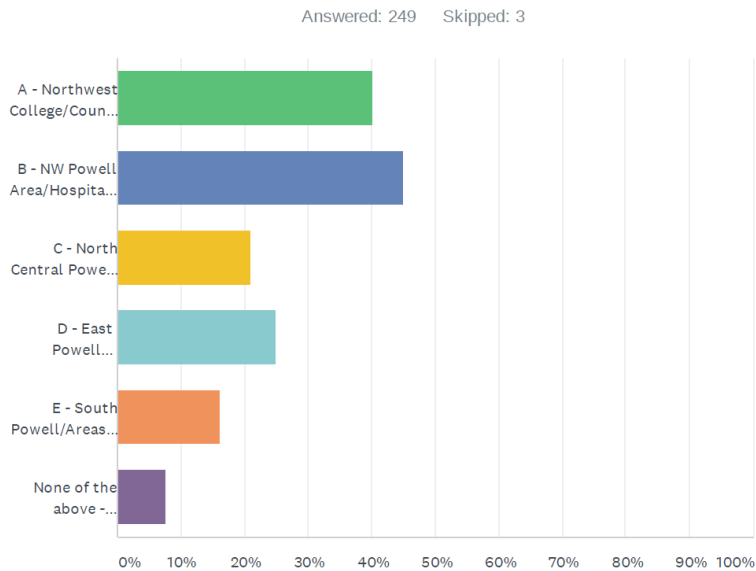
Q3 How often do you use the existing gravel pathway along Lane 8, and/or near the high school (see below)?



d. Neighborhoods (Question 4)

Most respondents are from the northwest portion of Powell, areas B and C of the figure above.

Q4 In which neighborhood(s) do you most frequently start and/or stop your bike, walking, or running journeys? (see image below)

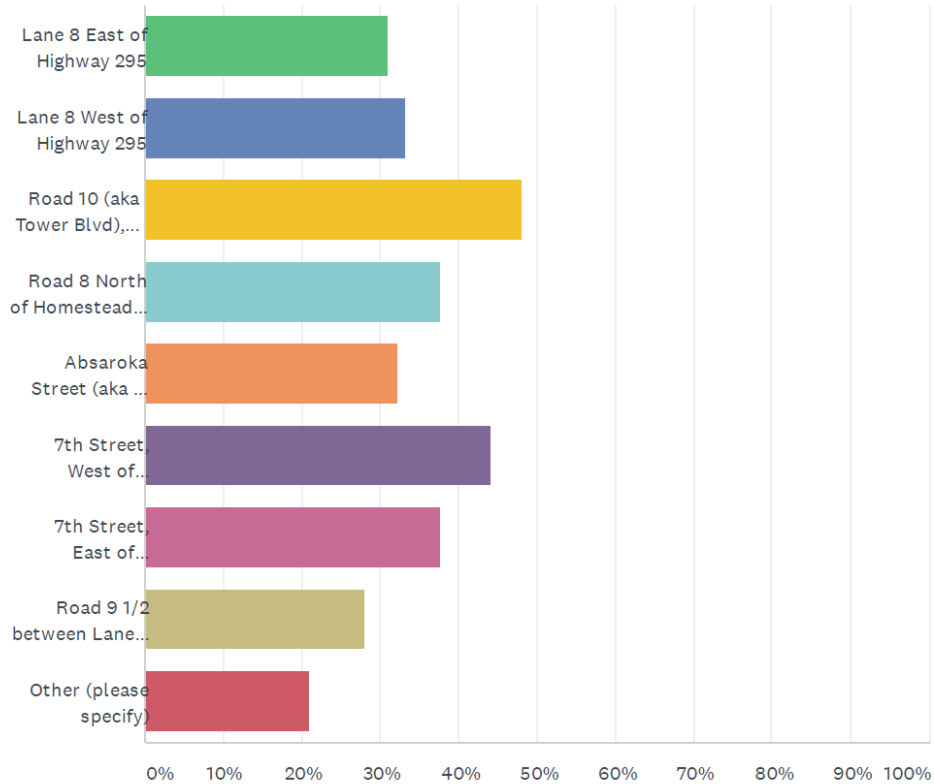


e. Preferred routes (Question 5)

Respondents preferred to walk, bike, or run along Road 10 (48%), Seventh St west of Absaroka (44%), Seventh St east (38%), and Road 8 north of Homesteader (38%). Other routes received less than 33.3% of responses.

Q5 When, walking running, or biking, I often follow the following routes (check all that apply):

Answered: 229 Skipped: 23



f. Concerns and comments (Question 6)

Respondents were able to provide any brief written response to this question, and responses varied with regard to topic, location, and concern. Major themes of responses include, in no particular order:

- Lack of separation from traffic, either due to no shoulder, no pathway, or narrow roadways.
- Concerns with traffic and the operation of vehicles, especially speed or inattentive drivers.
- County roads (Road 8, Road 10, Lane 8) are frequently specifically identified as feeling dangerous due to high speeds and no separation.
- Sidewalks in poor condition, discontinuous, or not present.
- Difficulty crossing busy streets, particularly Coulter Ave.
- The gravel surface of the existing pathways is unsuitable.

Existing Facilities

1. Existing Pathways and Bike Lanes

There are relatively few pathways or bike routes within the City of Powell. Existing routes are shown on Exhibit 1. A field reconnaissance was performed by engineering staff, with detailed photos and descriptions of the routes shown in Appendix A. A brief summary of each location is provided here.

a. Lane 8 Pathway

An existing pathway is constructed along the north shoulder of Lane 8 between Road 8 and Hwy 295. It is variable in width, with commonly about 6-ft of usable width. The gravel surface does not meet accessibility standards. Since the path is constructed on the road shoulder, at grade with the roadway, there is no separation from traffic. When visited for this study, there was no white fog line at the edge of the roadway. The speed limit on the road is 45 mph. It appears to be constructed above an irrigation pipeline. It is signed to limit conflicts between bicyclists and pedestrians, but there is no crosswalk, “Share the Road” signage, or similar alerting vehicle traffic.

b. Highway 295 (North Absaroka St)

Along Hwy 295 (Absaroka St), there is a signed bikeway along the west shoulder of the highway. It is paved, with a usable width of about 10-ft. While it is constructed at grade with the highway, there is a narrow (4-ft +/-) asphalt paved shoulder between the highway lane and the bike lane. The speed limit on this section of the road is 25-mph, but observed speeds are noticeably higher, as it is on the edge of town. Anecdotally, traffic is regularly observed driving onto the shoulder and bike lane during winter conditions, using it as a turn lane into the residential streets west of the highway.

c. Lateral C Pathway

A broad gravel pathway runs from east Seventh Street near the water tower, northwest to Lane 8, east of the north end of Gilbert Street. The pathway follows irrigation Lateral C, which was converted from open ditch to pipeline in the mid-1990s. It is signed for no motorized users. It is over 12-ft wide, but the gravel surface does not meet accessibility standards. With no adjacent roadway, there are no traffic conflicts along the length. A signed crosswalk, with road narrowing, provides a suitable crossing to Homesteader Park at the south end. At the northern terminus, pathway users must cross Lane 8 (45-mph) to access the exiting pathway on the north shoulder described above. At Lane 8, there is no crosswalk or signage alerting motorists.

d. Seventh Street

Seventh Street west of NWC is of variable width, with discontinuous sidewalk only on the south side. On the north side, the street shoulder is, in some locations, widened with gravel, likely above an existing irrigation pipeline. Pedestrians and bicyclists use that wider shoulder in lieu of an actual pathway or sidewalk.

East of NWC, Seventh Street typically has a sidewalk at the curb on both sides of the street through to Road 8 / Panther Blvd. On-street parking is common east of Bent Street, and there are no painted bicycle lanes at the time of this report.

Pedestrian crossings along Seventh Street through the NWC campus are outside the scope of this report.

2. Roads without Pedestrian Facilities

a. Road 10 / Tower Blvd

Road 10 is a heavily used county road, which sees significant truck traffic. Farm, commercial, and light vehicle traffic avoiding Powell on the west side of the City frequently use Road 10. While it is signed at 45-mph, traffic entering the study area from out of town is regularly observed traveling at higher speeds. It has no sidewalk, bike path, bike lane, or other pedestrian facilities. Bitter Creek runs parallel with the road on the east side, taking the shape of a large drainage ditch, and complicates future road widening or pathway improvements.



Figure 4 - Farm haul truck on Road 10 south of Lane 8. Note lack of shoulder, lack of striping, lack of sufficient space for non-motorized users. Signed 45-mph. Drainage ditch to east (right). Photo looking north.

Within this study area, Road 10 has no shoulders and is constructed on a low embankment that prevents bike or pedestrian use except in the traffic lane. The roadway is regularly used by bicyclists, runners, walkers (including strollers), and other recreationalists. With the narrow width and relatively high speeds, it presents a safety hazard to non-motorized traffic.

b. Lane 8 West of Hwy 295

Lane 8 between Road 10 and Highway 295 (Absaroka St) is a county road, and, like Road 10 above, is frequently used by both truck traffic and bikes/peds. While it has more shoulder in some locations, particularly on the north side, like Road 10 it has little room for separation of vehicular and non-vehicular roadway users.

Bitter Creek also runs along the south side of a portion of that section of Lane 8.

c. Road 8 / Panther Blvd

Road 8 (Panther Blvd within City limits) is a county road. It has wider shoulders, and in a few discontinuous locations, a sidewalk. In most locations, though, it lacks appropriate separation between bikes/peds and vehicle traffic. In and around the high school and football field, adequate accessible access is provided on the west side of the road.

3. Major Intersections

Most intersections in the study area have limited or no facilities or traffic control specifically for pedestrians and bicyclists. Intersections are stop sign-controlled, typically on one roadway only. Pedestrians crossing roadways without a stop sign are at greater risk.

4. Traffic Counts and Speed Data

Park County collects traffic data including count and speed on County-maintained roads. The most recent data available for the study area is from 2019. Traffic count locations are shown in the figure below. Traffic counts are conducted in the early summer months of May, June, and July. As a result, they do not reflect conditions during harvest, when farm truck traffic would significantly boost the percentage of trucks on the roadways.

Results of the traffic counts are shown in Table 1. From this data, the following observations are made:

- Traffic counts are higher towards US14A.
- Traffic speeds are highest on Road 10 north of Seventh St, and on Lane 8 near Road 10.
- Traffic speeds are reasonable for the posted limits. Also, very few comments have been received about the speed of vehicles within the study area. Far more comments have been noted about the volume of traffic, size of vehicles (trucks) and lack of separation, than about speed.
- Road 9H has a much lower volume of traffic than adjacent roads.

Table 1 - Traffic counts on County roads in study area (2019)

Road	Location	Avg Daily Trips (2019)			Speed (mph)	
		Cars	Trucks	Total	50%	85%
Road 10	North of US 14A (Coulter Ave)	1,608	50	1,658	37.0	42.1
Road 10	North of Lane 9	1,160	32	1,193	34.1	38.7
Road 10	South of Lane 8, North of 7th St	622	16	639	40.0	45.9
Lane 8	East of Road 10	714	20	734	42.3	48.5
Road 9H	South of Lane 8	309	7	317	34.8	42.6

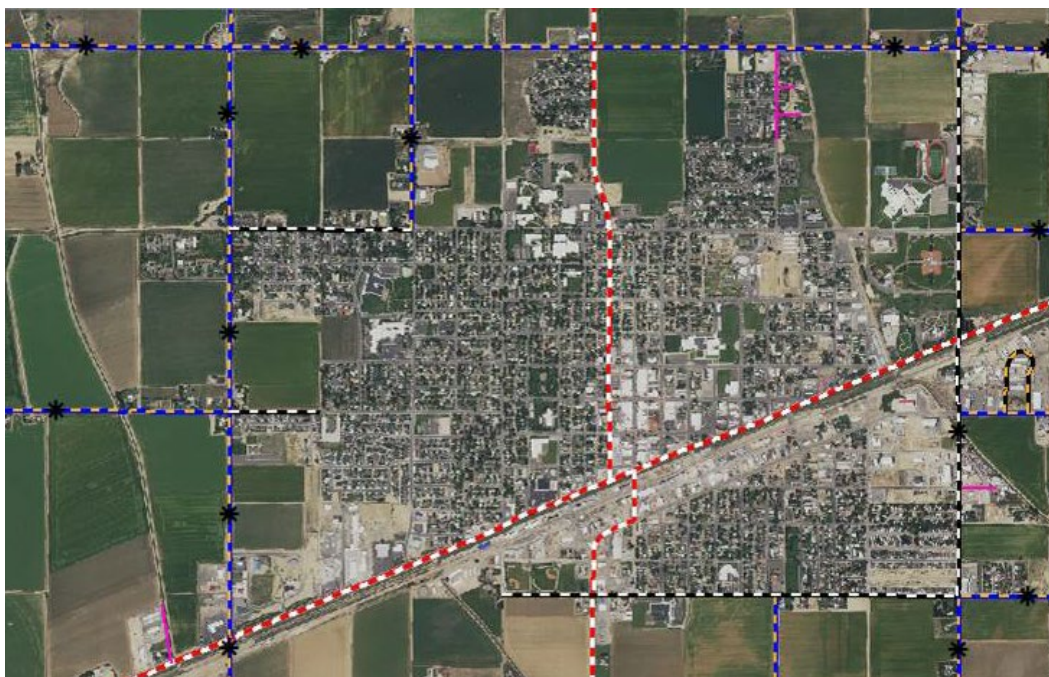


Figure 5 - Traffic count locations on County roads, indicated with black asterisk.

Alternatives

1. Summary

Alternatives considered here include improving existing pathways; constructing entirely new pathways; and improvements to existing pedestrian and bicycle facilities within the Powell city limits. An overview of these alternatives throughout the study area is shown in Exhibit 2. Closer views of the proposed improvements are shown in Exhibits 3 and 4, respectively showing the northeast and northwest areas in and around Powell. A brief written description of each alternative is provided here. Please refer to the photos and written descriptions shown in Appendix A for information about the sites' current conditions and constraints.

The layout shown below was used as a typical design when estimating costs for proposed new pathways along county roads/lanes for this study. Conceptually, new pathways are proposed with a 10-ft wide smooth asphalt surface, over a crushed gravel base, with pitrun subbase where necessary.

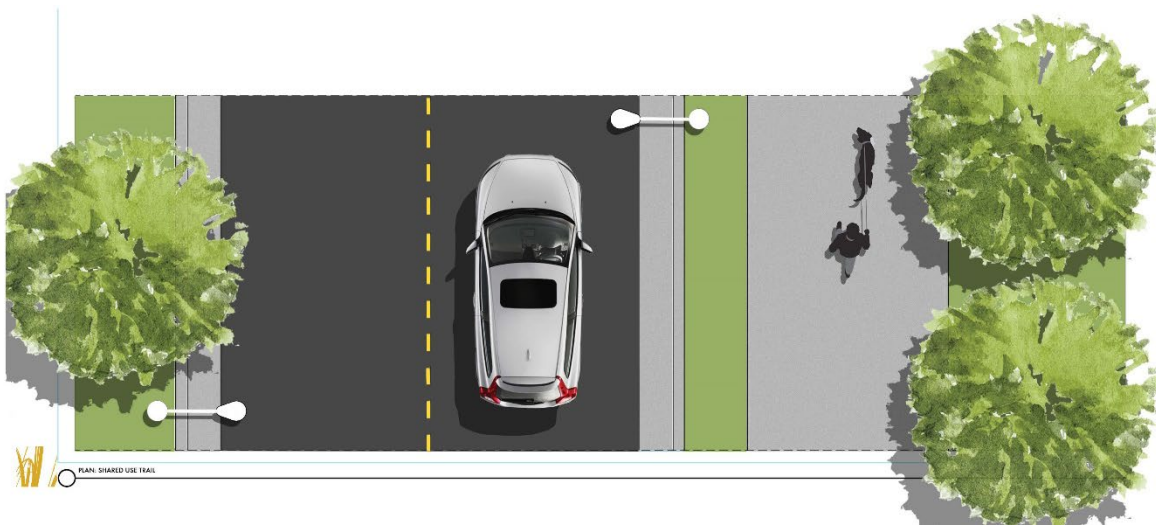


Figure 6 - Shared use trail adjacent to street with 4-ft separation (Tallgrass Landscape Arch.).

Alternatives that required extensive additional right-of-way width were not considered further, with the principal reason being a lack of funding for land acquisition. However, land with county road frontage north of city limits is largely undeveloped. Requiring construction of pathway, or provision of an easement to allow future construction of a pathway could conceivably be required during the subdivision process. The regulations to do so do not currently exist in Park County. Within the City of Powell, land for a pathway could be considered as parkland (which is required) if the governing bodies and developer agreed.

2. Improvements to Existing Pathways

The existing pathways, while beneficial to the community, are lacking important safety or accessibility features. To that end, the two existing pathways should both be paved, and receive new signage and safety improvements. Gravel surfaces do not meet ADA accessibility requirements, and they are not readily used by many bicyclists or other wheeled pathway users.

a. Lateral C Pathway

The existing Lateral C pathway, running from Homesteader Park at Seventh St, to Lane 8, has an excellent layout and location. Its use is hampered by its soft gravel surface. Paving it would require only modest

improvements to the existing gravel section, and is the lowest-cost alternative considered. Few other improvements would be necessary to bring this pathway up to standard.

b. Lane 8 Pathway

The existing pathway along the north shoulder of Lane 8, east of Hwy 295, can also be readily paved to meet ADA requirements. As it is attached to the county road, it would have limited separation from vehicle traffic. Additional measures including signage, striping, and gravel shoulder would be necessary to keep pathway users safer. Several survey respondents commented that there was insufficient separation along this pathway given the traffic speeds.

East of the intersection with the existing Lateral C pathway, there is no pathway along Lane 8. However, there is sufficient space along the shoulder to construct an adjacent, at-grade pathway. An existing irrigation pipeline must be considered during planning and design.

On Lane 8 west of Hwy 295 (Absaroka St), there is no existing pathway. A new pathway could be constructed within the ROW on the north side of the lane, although a conflict with existing irrigation pipelines must be addressed in design and planning. As Lane 8 in this area is rural, traffic speeds are higher and separation from the existing roadway should be maximized.

3. Improvements to Facilities within City Limits

a. Highway 295 (Absaroka St)

Along the west side of Highway 295 (Absaroka Street), the existing pathway has limited to no separation from the shoulder. Where some separation (up to 4-ft) exists, there is limited demarcation and the pathway is not visually distinct from the travel way. In snowy conditions, traffic can be seen using the pathway as a turn lane or supposing it to be a four-lane street. Improvements to provide separation and visual demarcation are recommended. This might include semi-flexible demarcation posts, concrete flatwork, and/or landscaping.

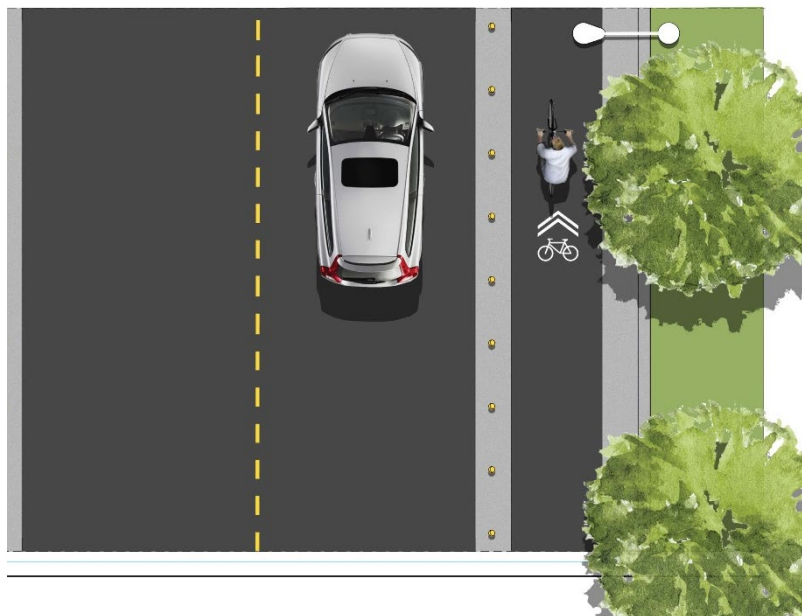


Figure 7 - Separation from traffic with at-grade bike path on shoulder (Tallgrass Landscape Arch.)

b. Seventh Street

Seventh Street west of Northwest College is a residential neighborhood, fully developed on the south side and partially developed on the north side. The existing sidewalk with curb and gutter is mostly continuous along the south side, although a few locations need ADA improvements. There is little to no curb, gutter, or sidewalk along the north side, but sufficient space exists to construct a pathway. Irrigation conflicts would be present.

This Street provides a necessary connection between the residential northwest corner of Powell to NWC and other areas in town. Furthermore, it connects NWC’s Trapper West housing area to campus. Seventh Street should be given high priority for in-town bicycle connectivity.

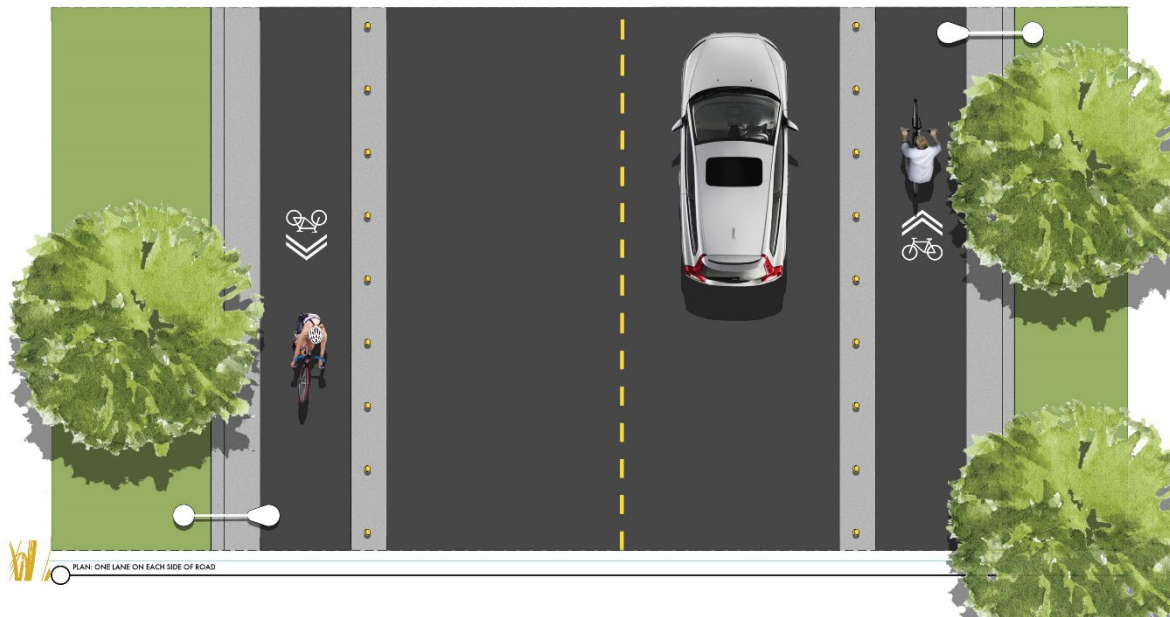


Figure 8 - Bike lane along traffic lane, each side. Sidewalk could be constructed behind curb. (Tallgrass Landscape Arch.)

East of NWC, the street is largely developed with mostly continuous curb, gutter, and sidewalk. The street is sufficiently wide for painting dedicated bike lanes, which would improve bicycle safety.

4. New Pathway Locations

Several locations would benefit from new pathways. For cost estimating purposes, new pathway was conceptually designed as a 10-ft wide asphalt pathway over a crushed gravel base course, with some imported pit run subbase where additional roadway embankment would be needed.



Figure 9 - 10-ft wide asphalt pathway in Teton County

a. Road 10 / Tower Boulevard

Survey respondents noted dissatisfaction with this location, feeling it to be unsafe and traffic speeds to be high. It does present many design challenges, though. Constructing a new pathway on the road embankment of Road 10 north of city limits would be difficult, and consideration should be given to alternate alignments. Shifting a proposed pathway either east or west of the existing right-of-way would require the purchase of land and/or easements. It would also require careful design to work around the existing large ditch of Bitter Creek. The connection would be made to Seventh Street at the south, which may require improvements at that intersection. The construction cost estimate for this location includes more money for ROW and/or easements than any other alternative.

b. Road 8 / Panther Boulevard

Road 8 on the east side of town, north of the high school, is well suited for a new pathway. The existing ROW has fewer apparent utility or irrigation conflicts, and the existing roadway is on only a low embankment, simplifying the construction of an adjacent pathway.

c. Road 9H

A new pathway running north-south along Road 9 ½ would effectively divert some pedestrian traffic away from Road 10, while still providing similar bike/ped access from the City to Lane 8. It has additional advantages of a lower existing roadway embankment; lower traffic speeds with limited through traffic; and fewer irrigation/drainage challenges, though they are still present. There are some private driveway crossings, and existing irrigation ditches that would need to be designed around.

This is expected to be a lower-cost option to provide safe separation between vehicles and pedestrians compared to improvements along Road 10. And unlike Road 10, it is not a designated truck route.

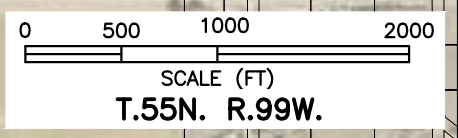
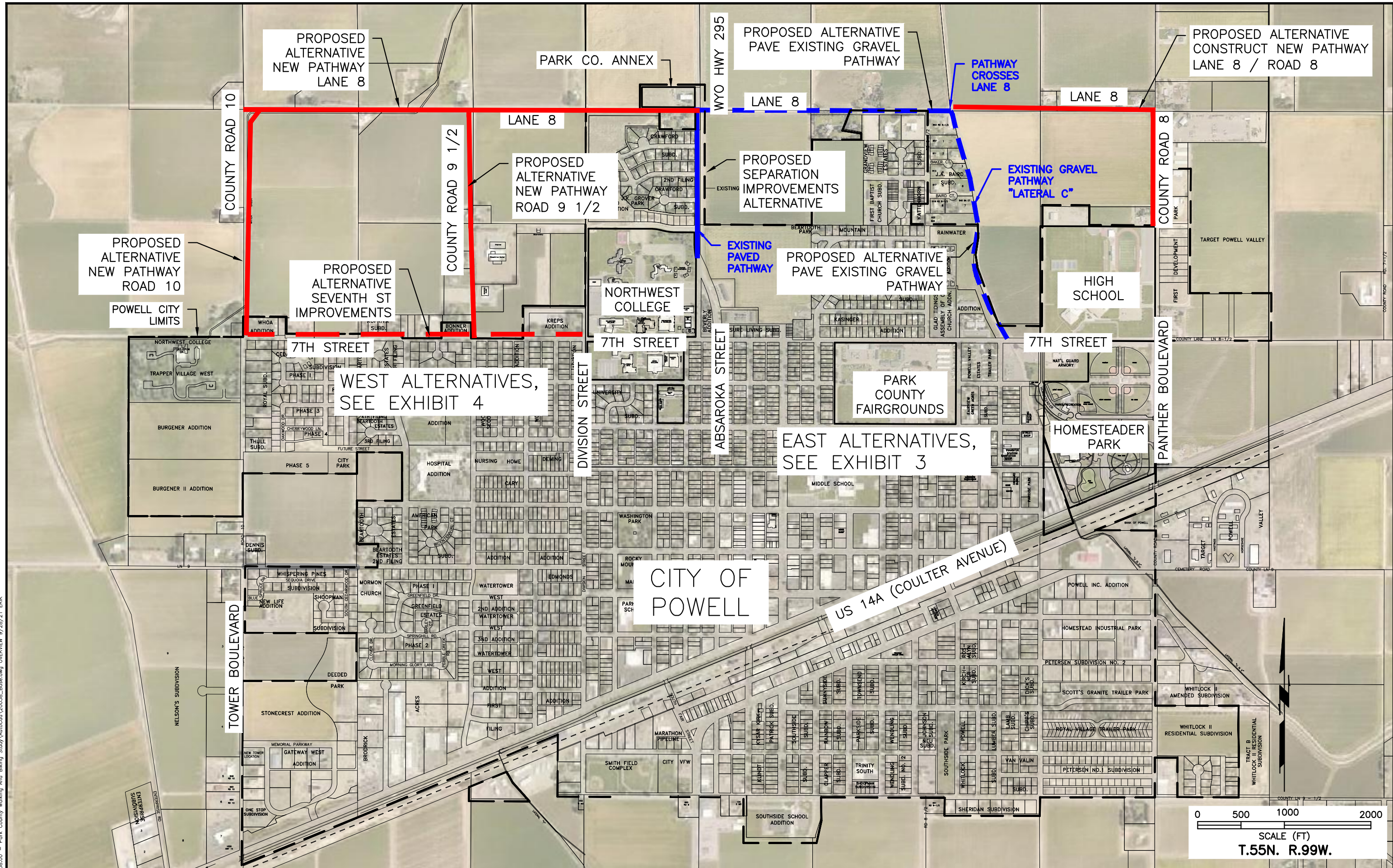
5. Conceptual Cost Estimates

A summary of the conceptual cost estimates is presented in Table 2 below. For detailed cost estimates, see Appendix C. Note that these estimates were prepared without the benefit of detailed construction design or surveying, and include a 20% contingency. Determining the cost of easements is also very uncertain due the need to negotiate with individual landowners. Easements were included in cost estimates where additional right-of-way width may be required or existing irrigation utilities may present a potential conflict with a proposed bike path. Finally, construction prices have increased dramatically during the period that this report was prepared. Whether the inflation rate remains high or returns to recent lower levels is uncertain.

Table 2 - Summary of Conceptual Construction Cost Estimates

Alternative	Description	Total Cost
Alt 1	Pave existing Lateral C pathway	\$ 133,590
Alt 2	Pave existing Lane 8 pathway without separation	\$ 149,661
Alt 3	Pave existing Lane 8 pathway with gravel separation	\$ 203,123
Alt 4	New pathway Lane 8 between Road 10 and Hwy 295	\$ 509,760
Alt 5	New pathway Road 10 between Seventh St and Lane 8	\$ 481,934
Alt 6	New pathway Road 9 1/2 between Seventh St and Lane 8	\$ 310,442
Alt 7	New pathway Road 8 from HS to Lane 8, and Lane 8 to existing	\$ 378,669
Alt 8	Pedestrian improvements along Seventh St	\$ 450,866
Alt 9	Traffic separation along Absarkoka St / Hwy 295	\$ 317,546

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DATE	DRAWING LOG	BY	CHECKED	APPROVED
09/28/21	FINAL REPORT	LPU	EDW	TLC

DRAWN BY: LPU
 JOB NO. 20038.00
 FIELD BOOK NO. OFFICE

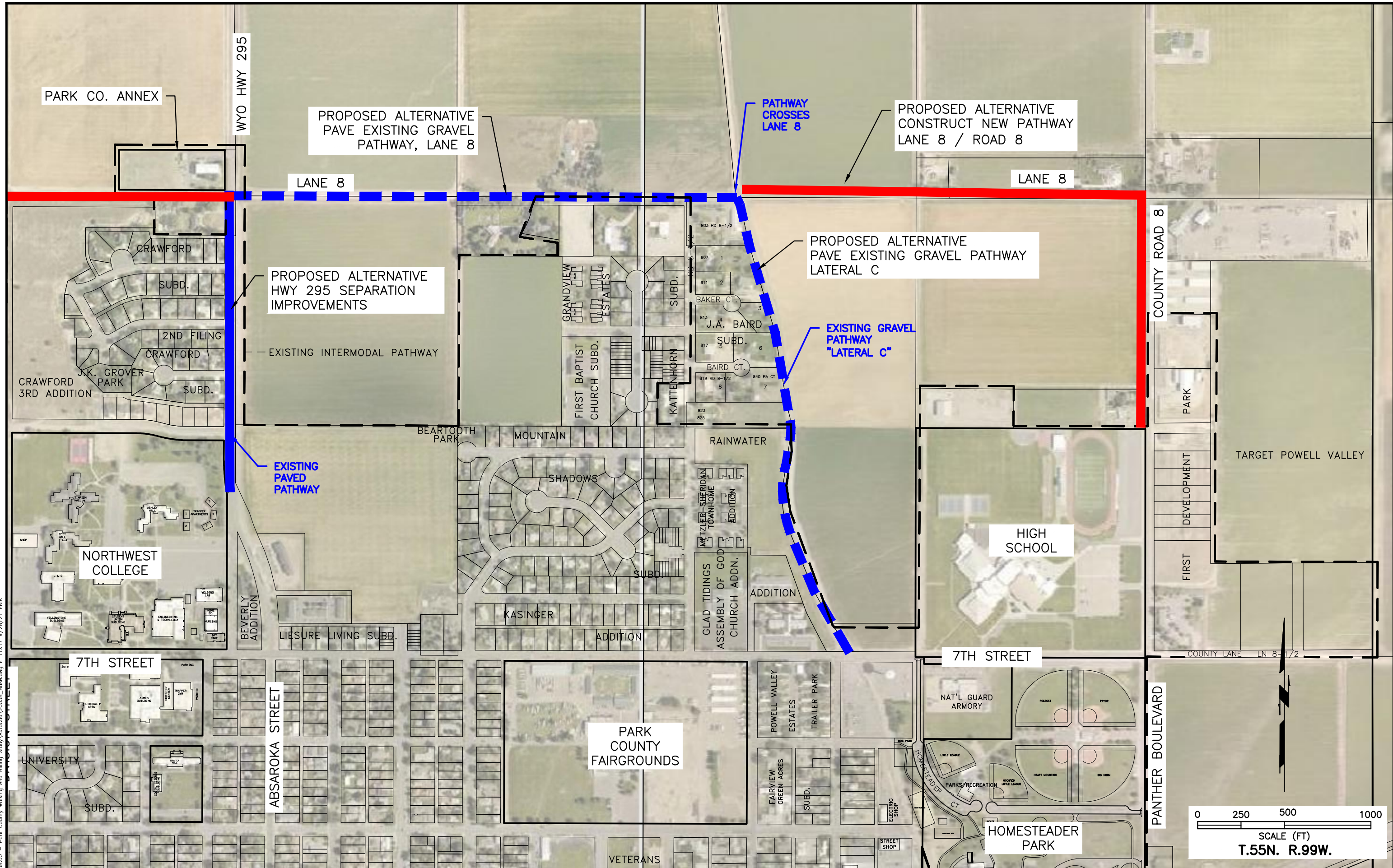
**ENGINEERING ASSOCIATES
 CONSULTING ENGINEERS & SURVEYORS**

OWNER: **PARK COUNTY**

PROJECT: **NORTH POWELL PEDESTRIAN AND BICYCLE FACILITIES**
 TITLE: **OVERVIEW OF PATHWAY ALTERNATIVES**

EXHIBIT 2

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DATE	DRAWING LOG	BY	CHECKED	APPROVED
09/28/21	FINAL REPORT	LPU	EDW	TLC

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 JOB NO. 20038.00
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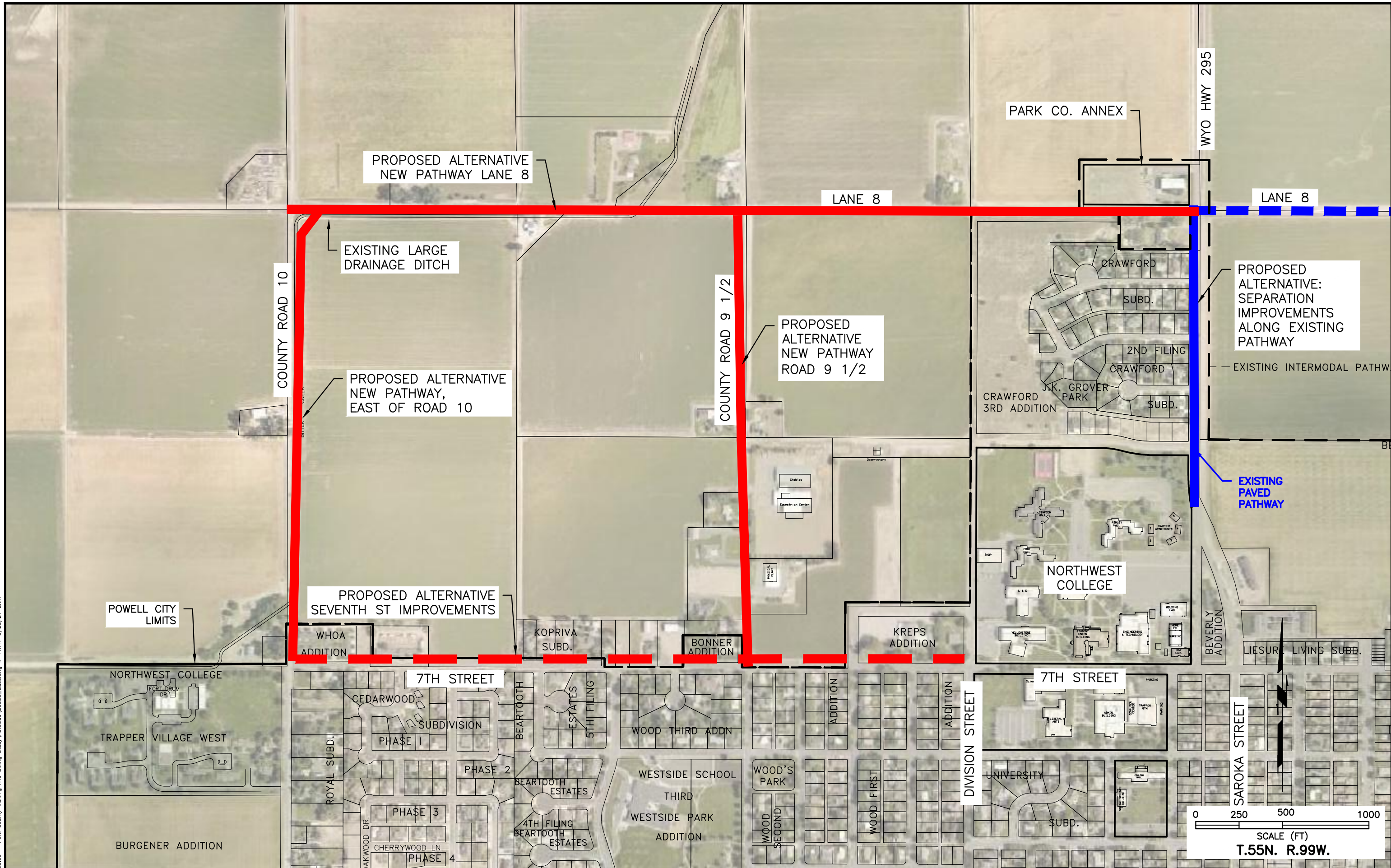
ENGINEERING ASSOCIATES
CONSULTING ENGINEERS & SURVEYORS

OWNER: **PARK COUNTY**

PROJECT: **NORTH POWELL PEDESTRIAN AND BICYCLE FACILITIES**
 TITLE: **PATHWAY IMPROVEMENT ALTERNATIVES (EAST)**

EXHIBIT 3

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DATE	DRAWING LOG	BY	CHECKED	APPROVED
09/28/21	FINAL REPORT	LPU	EDW	TLC

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 FIELD BOOK NO. OFFICE

**ENGINEERING ASSOCIATES
 CONSULTING ENGINEERS & SURVEYORS**

OWNER:
PARK COUNTY

PROJECT: **NORTH POWELL PEDESTRIAN AND BICYCLE FACILITIES**
 TITLE: **PATHWAY IMPROVEMENT ALTERNATIVES (WEST)**

EXHIBIT 4

Recommendations

1. Need for Improvements

From survey results, and the field reconnaissance performed for this study, it is clear that there is insufficient separation between vehicles (including trucks), and bicyclists and pedestrians on the north side of Powell. This problem was frequently mentioned along Road 10 (Tower Blvd), and includes Lane 8.

The large number of survey respondents indicates community concern about traffic and pedestrian safety in the area.

This area is also relatively undeveloped. Constructing pathways now will be lower cost than retrofitting pathways into the ROW after further development. Alternatively, it is possible (though not currently in regulations) that property developers in the area could be required to construct pathway as a condition of development. That route is not recommended for technical and regulatory reasons.

The existing pathways are under-utilized because their gravel surface is difficult for bicyclists and other wheeled users. This can be remedied with a relatively simple paving project.

2. Recommendations for Further Study or Design

It is recommended that the following options be advanced for further consideration and/or action:

- For the existing pathways along Lane 8 and Lateral C, improve the existing alignment with asphalt surfacing, signage, and traffic separation barriers (or other measures).
- Construct a new pathway north from Powell to Lane 8. This could be along Road 9 ½ from Lane 8 south to Seventh St. This route could serve as the necessary north-south connector and provide an option off of Road 10. Alternatively, construct a new pathway along Road 10.
- Extend a new pathway along Lane 8 west of the County Annex.
- Provide bike lane, parking demarcation, signage, and consistent sidewalk along Seventh Street through the entirety of City limits.

Stakeholder Input

Following the circulation of this report in draft form, a meeting with community stakeholders was held. The purpose of the meeting was to obtain input from various affected parties for inclusion in this final report. The meeting was held on November 10, 2021, in the council chamber at Powell City Hall. The agenda and sign-in sheet for the meeting are included in Appendix E.

Prior to that meeting, the draft report was also presented to the Park County Commission at its regular business meeting on October 19, 2021. The minutes of that meeting are also included in the appendix.

1. WYDOT

WYDOT is supportive of projects that increase pedestrian and driver safety. They are the primary funding agency for projects of this sort. WYDOT staff answered questions regarding funding options for pathway projects during the stakeholder meeting. This information is summarized in the Project Funding section of the report below.

2. City of Powell

City of Powell staff has provided input on bike routes and pedestrian improvements. Staff has indicated that the City supports pedestrian safety, but is wary of increased maintenance expenses should pathway

improvements fall within City limits. The City has not indicated an interest in being the sponsor or debt holder if pathway projects are financed with TAP funds.

3. Northwest College

NWC staff is supportive of bicycle and pedestrian pathways that provide connectivity with other locations in town, including the Trapper Village West neighborhood. Staff report that City residents and other non-student community members regularly make use of NWC grounds for dog walking, exercise, or pedestrian through-traffic. Staff would like to see improved or new pathway at the northeast corner of the campus and pedestrian/bike facilities along Seventh Street west of campus.

Most of the pathway alternatives considered west of WYO295 would require coordination with either the College, or the Northwest College Foundation, as landowners. In particular, any pathway constructed along Road 9H would require coordination with either NWC or the Foundation.

4. Shoshone Irrigation District

Shoshone Irrigation District (SID) was unable to attend the report review meeting.

Many of the proposed alternatives would be co-located with irrigation infrastructure. In many locations of the study area, rights of way appear on the surface to have sufficient space for new pathway. However, these spaces are often clear on the surface because there are buried irrigation or drain pipeline(s) below. Maintaining unimproved space above the irrigation pipeline or around ditches can provide separation space from vehicles where sufficient ROW can be obtained outside the existing pipeline alignments. Careful design would be required around diversion boxes, drain structures, and other surface improvements.

Typical irrigation maintenance practices, including unplanned maintenance, disturb the ground around ditches and buried pipelines. If a new pathway is constructed above irrigation pipeline, provision must be made by the pathway owner to reconstruct the asphalt pathway surface following pipeline excavation. Furthermore, vehicle access (including farm access crossings) must be considered in pathway design to prevent damage to asphalt paving by irrigation district vehicles, or by heavy farm equipment.

5. Park County School District #1

PCSD1 staff attended the stakeholder meeting. District staff support projects that improve connectivity and keep children walking to school out of traffic. The southern terminus of the Lateral C pathway, adjacent to the High School, lacks sidewalk connection to the school grounds to the east.

6. Powell Economic Partnership

PEP staff attended the stakeholder meeting, and commented on the desirability of pathways as a community asset. In addition, PEP suggested adding educational components along pathways that inform pathway users about agricultural history, practices, and value to our community, while reducing conflicts between pathway users and agricultural landowners.

7. Local Landowners

Mr. Tom Churchill of Churchill Farms spoke on behalf of farming interests on the north side of Powell. He farms several parcels adjacent to City limits, both owned and leased. He noted conflicts with bicyclists and pedestrians, including dog walkers, in numerous locations within the study area. This includes areas with pathway (Lateral C) and areas without (Road 10 and Lane 8).

Mr. Churchill is generally opposed to the construction of new pathways, and shared the following concerns:

- He is opposed to the encroachment of suburbia into agricultural land.
- He is opposed to improvements that will bring additional pedestrians into agricultural areas.
- Some pathway users are disrespectful of private property and agricultural operations.
- Construction of new pathways leads to a loss of farmable ground.
- Construction of new pathways cause the following problems for farming activities:
 - They make access to and maintenance of irrigation improvements more difficult;
 - Farming operations more constrained; and
 - They limit access to farm fields by tractors, equipment, and trucks.
- It is difficult to maintain pathways, and the margins around pathways. This includes the ground surface, gravel, etc., but also dog waste and litter.

Available Funding

Federal grant money is available for pathway projects through the Transportation Alternatives program, administered by WYDOT. This grant program is 80/20 split between Federal grant and local match dollars. Funding for the alternatives presented here is not guaranteed, and the program is competitive. In FY2022, WYDOT received applications totaling more than \$5 million, with about \$2.3 million in available funding. A state selection committee reviews applications and makes awards based on the funding available from year to year.

To participate in the program for the fiscal year 2023, the applicant must provide WYDOT with a statement of intent due before April 15, 2022. A meeting with WYDOT will follow. A full application is due by July 15. The committee makes awards in November.

The maximum award through the program is \$500,000. Including the minimum 20% match, the maximum budget is \$625,000. However, to complete a project with a larger total cost, the project can be phased over multiple years through completion. As an example, a project might be phased into three parts. The first phase would be design and permitting, including all pathways included within the project. Construction would be funded beginning with the second phase, up to a maximum of \$500,000 grant with a minimum 20% match. Construction could continue the following year with a third phase with the same constraints. Once a project has been fully designed and permitted (as in the first phase of a multi-year project), the “shovel-ready” status is viewed favorably when considering funding applications.

If a project were awarded funding, design and permitting could begin in 2023, with completion in that calendar year. Construction could begin in a subsequent phase in 2024.

Conclusions

As a result of this study, including stakeholder and community input, the following conclusions are offered.

- On the northern side of Powell, narrow county roads serve as important truck routes and see frequent use by people walking, running, or riding bicycles.
- Significant community interest exists for solutions that reduce vehicle-pedestrian conflict.
- Constructing new pathways along county roads will provide additional separation between vehicles and non-motorized users.
- Locations of proposed pathways are constrained by roadway embankments, subsurface utilities, irrigation ditches, pipelines, and drains. Additional right-of-way or easement will be required, though to what extent remains undefined.
- Pathway design should be handicap accessible, meeting ADA requirements.
- Pathways should be separated from vehicle traffic to provide the safest option for all.
- Construction of pathway is more cost-effective prior to residential or other development than it is during or after development.
- Farmers are seeing an increase in the number of bikers and pedestrians within the study area, bringing both safety concerns and difficulties in ag operations. As the City continues to grow, this trend is expected to continue.
- Agricultural operations, including irrigation and on-farm practices, must be accommodated during design and construction.
- Constructing a new pathway in Road 10 and Lane 8 will reduce safety risks along the signed truck route.
- Funding opportunity exists through WYDOT's Transportation Alternatives Program, with an 80/20 grant/match split. Funding is competitive. A large project must be phased.

Appendix A – Existing Facilities Field Reconnaissance

East Seventh Street near High School



Looking east



Looking west

Notes: Sufficient room for bike lane on street, or pathway off street, with minor surface improvements. Significant pedestrian and bicyclist use in area after school.

Existing Gravel Pathway between Seventh St and Lane 8



Looking north



Looking south

Notes: Existing pathway is gravel surface, doesn't meet ADA requirements. Otherwise excellent alignment, suitable for improvement.

Lane 8 West of Road 8



Looking west

Notes: No pathway exists. Sufficient space on north shoulder for construction of bike lane.

Lane 8 East of WY 295 / Absaroka Street



Looking east



Looking west

Notes: Existing gravel pathway on north shoulder. Could be improved / paved. Currently no separation from traffic, 45-mph speed limit.

Lane 8 West of WY 295 / Absaroka St, at County Annex



Looking east in front of Annex



Looking west in front of Annex

Notes: Existing bike path on Lane 8 east of WY295 ends at intersection. Sufficient space available on north should of Lane 8 west for pathway.

Lane 8 West of WY 295 / Absaroka St, West of County Annex, to Road 9H



Looking west, west of Annex

Notes: No existing bike path. Sufficient space on north shoulder for improvements. Existing irrigation pipeline beneath surface.

Lane 8 between Road 9H and Road 9



Looking west

Notes: Shoulder to south constrained by drainage ditch. Existing irrigation pipeline running beneath north shoulder.

Road 9H South of Lane 8



Looking south, near NWC facilities



Looking south, near Lane 8

Notes: North of developed area, a pathway could be readily constructed on east shoulder, sharing space with what appears to be an existing irrigation pipeline. Much less traffic on Road 9H than on Road 10. Within developed area, right of way is more constrained but a sidewalk-width pathway could be planned.

Road 10 between Lane 8 and Seventh Street



Looking north. Note width of beet-haul truck in comparison to total road width.

Notes: Signed truck route, unsuitable for pathway / pedestrians without significant improvements to roadway. Elevated roadway section, large drainage ditch on east shoulder. Site visit made Oct. 14, 2020, at peak of beet hauling. Numerous semi-trucks observed using full width of lanes.

Road 8 Between Homesteader Park and Lane 8



Looking north from intersection at 7th St



Looking south from near Lane 8

Notes: North of the high school, there is sufficient space for a pathway on either the east or west shoulders, with narrowing around driveways.

Seventh Street between Road 10 and Division St



Looking east from near Road 10



Looking west from near Mtn View St

Notes: Sidewalk is constructed along the south side of 7th Street through its entire length. Space exists on the north side for bike path or sidewalk, though an irrigation pipeline appears to be buried along the north shoulder.

Absaroka St (WY295) between 7th St and Lane 8



Looking south near Bald Ridge Dr



Looking south from intersection with Lane 8

Notes: Existing bike lane along west side of Absaroka, with signage. No sidewalk, curb, or separation/barriers to traffic other than paint.

Extension of Division Street, north of 7th and NWC campus and south of Lane 8



Looking north near soccer fields.



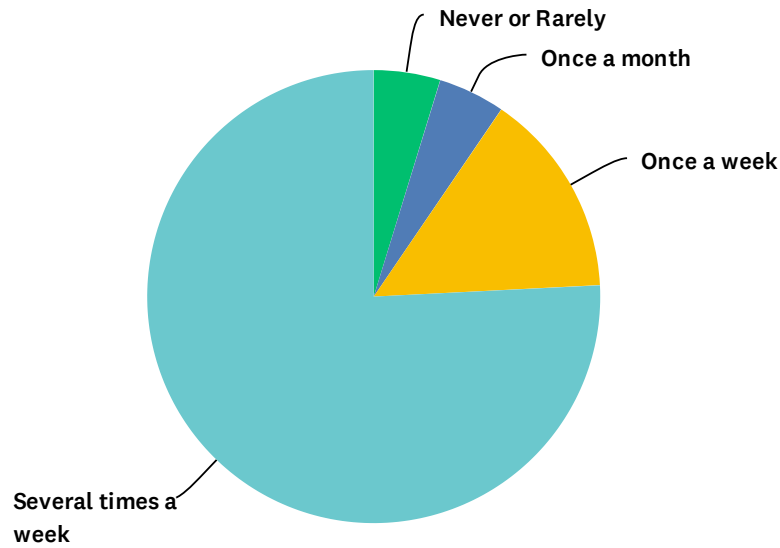
Looking south near Lane 8

Notes: Through campus, sufficient space for pathway along west side adjacent to soccer fields. North of campus, farm access road could be improved on east side to accommodate pathway.

Appendix B – Community Survey Results

Q1 When exercising outdoors or commuting, how often do you ride a bike, walk or run?

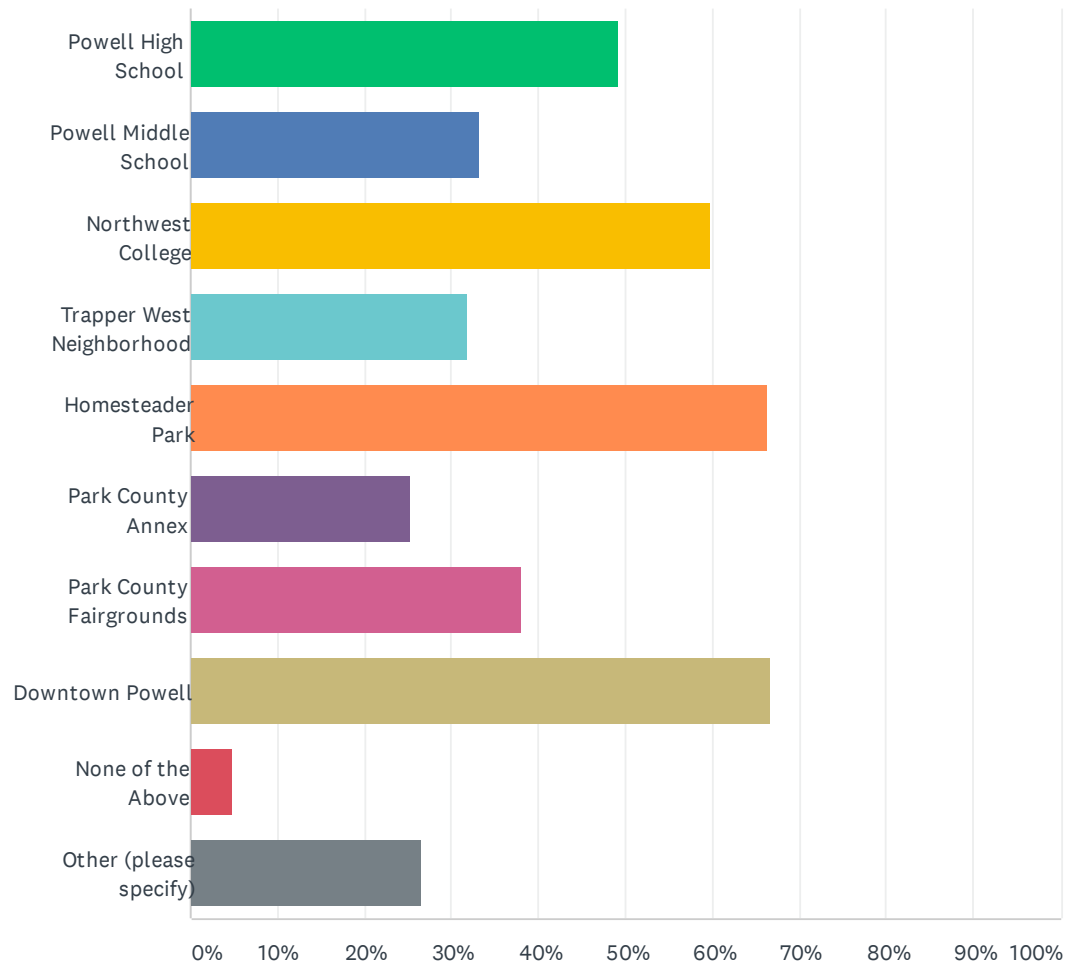
Answered: 252 Skipped: 0



ANSWER CHOICES	RESPONSES	
Never or Rarely	4.76%	12
Once a month	4.76%	12
Once a week	14.68%	37
Several times a week	75.79%	191
TOTAL		252

Q2 In the last year, to which of the following destinations/area have you or someone in your household walked or ridden on a bike?

Answered: 252 Skipped: 0



ANSWER CHOICES	RESPONSES	
Powell High School	49.21%	124
Powell Middle School	33.33%	84
Northwest College	59.92%	151
Trapper West Neighborhood	31.75%	80
Homesteader Park	66.27%	167
Park County Annex	25.40%	64
Park County Fairgrounds	38.10%	96
Downtown Powell	66.67%	168
None of the Above	4.76%	12
Other (please specify)	26.59%	67
Total Respondents: 252		

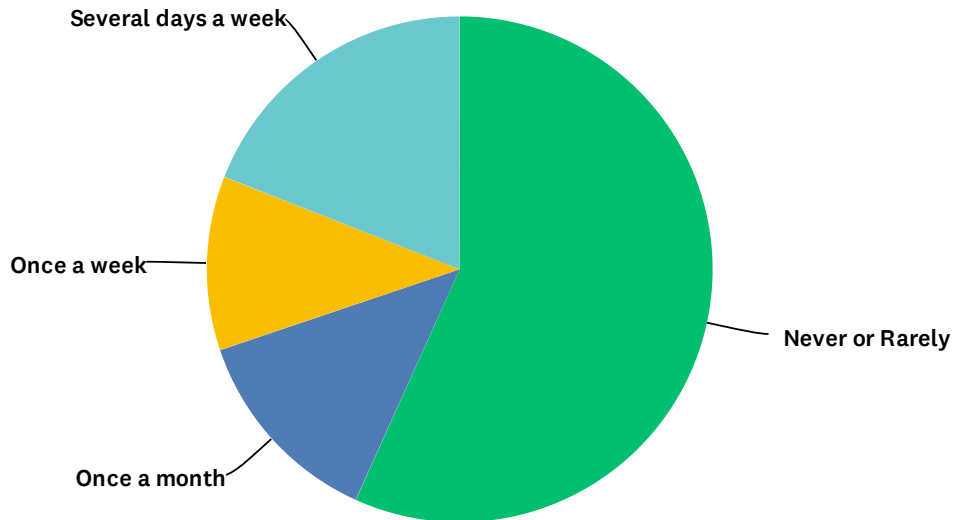
#	OTHER (PLEASE SPECIFY)	DATE
1	West side school	5/25/2021 4:18 PM
2	Dog park	5/19/2021 7:43 PM
3	Blair's & Mr D's	5/10/2021 8:02 PM
4	West side elementary	5/5/2021 8:23 PM
5	Wyoming highway 295	4/28/2021 4:12 PM
6	Wyoming 295 highway	4/28/2021 4:12 PM
7	Westside School	4/13/2021 1:41 PM
8	I ride everywhere for excercise.	3/27/2021 2:30 PM
9	Parkside Elementary	3/17/2021 12:33 PM
10	Ave E, Airport road	3/13/2021 11:21 PM
11	Westside Elementary	3/7/2021 5:01 PM
12	Out of town on the highway	3/5/2021 11:01 AM
13	Fishing access off of Road 9, Road 5 area	2/26/2021 11:53 AM
14	Polecat Bench	2/26/2021 9:53 AM
15	City streets and county roads	2/18/2021 7:38 AM
16	Westside	2/11/2021 5:46 PM
17	Monroe Street, Road 8 1/2 out of town	2/10/2021 10:07 AM
18	Woods Park	2/9/2021 8:42 AM
19	I WALK from gilbert st west to road 10 south to 7th st and back	2/8/2021 2:58 PM
20	Bike path from lane 8 to Dog Park	2/6/2021 10:29 PM
21	Out Avenue E	2/6/2021 4:19 PM
22	I walk primarily on farm roads.	2/6/2021 1:06 PM
23	Ave E	2/5/2021 7:28 PM

24	usually to the gym or to access Ave E and the Powell/Cody Hwy	2/5/2021 7:15 PM
25	Road 10, Shopko area, hospital area	2/5/2021 6:42 PM
26	Family Dollar	2/5/2021 4:35 PM
27	Beck Lake walking paths	2/5/2021 7:02 AM
28	Westside elementary	2/5/2021 12:26 AM
29	South side of Rd 8H	2/4/2021 10:54 PM
30	County roads	2/4/2021 9:54 PM
31	Blair's/Family Dollar	2/4/2021 9:41 PM
32	Washington park	2/4/2021 8:11 PM
33	Downtown/Bent St	2/4/2021 7:15 PM
34	Out lane 8	2/4/2021 5:38 PM
35	South on road 9 past the willwood irrigation center and along the canals	2/4/2021 5:20 PM
36	Work, south of town on Road 9	2/4/2021 4:47 PM
37	Washington park	2/4/2021 4:41 PM
38	Lane 11 1/2, road 11, lane 11, road 10	2/4/2021 4:41 PM
39	Northwest neighborhoods	2/4/2021 4:28 PM
40	Avenue E	2/4/2021 3:51 PM
41	Areas around Westside School & hospital to downtown	2/4/2021 2:55 PM
42	Lane 8 around by college then downtown, also lane 8 then high school by Homestead Park down down then back to Katenhorn	2/4/2021 2:50 PM
43	Normal walk route, Ave E to Road 10, Road 10 to 7th St, 7th St to Division and back to Ave E	2/4/2021 12:41 PM
44	Around our neighborhood & southside school approx 2 miles each walk	2/4/2021 12:39 PM
45	Road 8	2/4/2021 12:24 PM
46	Walked lots of different places! I try to walk every day, summer and winter. I also ride my bike in good weather.	2/4/2021 11:33 AM
47	Westside Elementary	2/4/2021 7:36 AM
48	Hospital	2/2/2021 4:53 PM
49	Division to Coulter and back	2/1/2021 10:13 PM
50	Westside Park	2/1/2021 6:51 PM
51	Legion baseball fields	2/1/2021 5:37 PM
52	Bench and on roadways	2/1/2021 2:39 PM
53	Grocery Stores	1/31/2021 4:41 PM
54	Washington Park	1/31/2021 3:10 PM
55	Open field north of Powell	1/31/2021 1:56 PM
56	Southside elem	1/31/2021 1:14 PM
57	Grocery stores	1/27/2021 3:07 PM
58	The Powell cemetery	1/27/2021 9:57 AM
59	Southside	1/27/2021 8:08 AM
60	Ave E, Greenfield area	1/26/2021 8:10 AM

61	Outskirts of Powell	1/26/2021 8:08 AM
62	Westside, washington	1/25/2021 10:27 PM
63	Beck Lake	1/25/2021 8:22 PM
64	Outside city limits and cemetery	1/25/2021 7:44 PM
65	County roads on the W, N and E sides of town	1/25/2021 7:21 PM
66	Outside the city limits	1/25/2021 7:14 PM
67	Westside	1/25/2021 6:06 PM

Q3 How often do you use the existing gravel pathway along Lane 8, and/or near the high school (see below)?

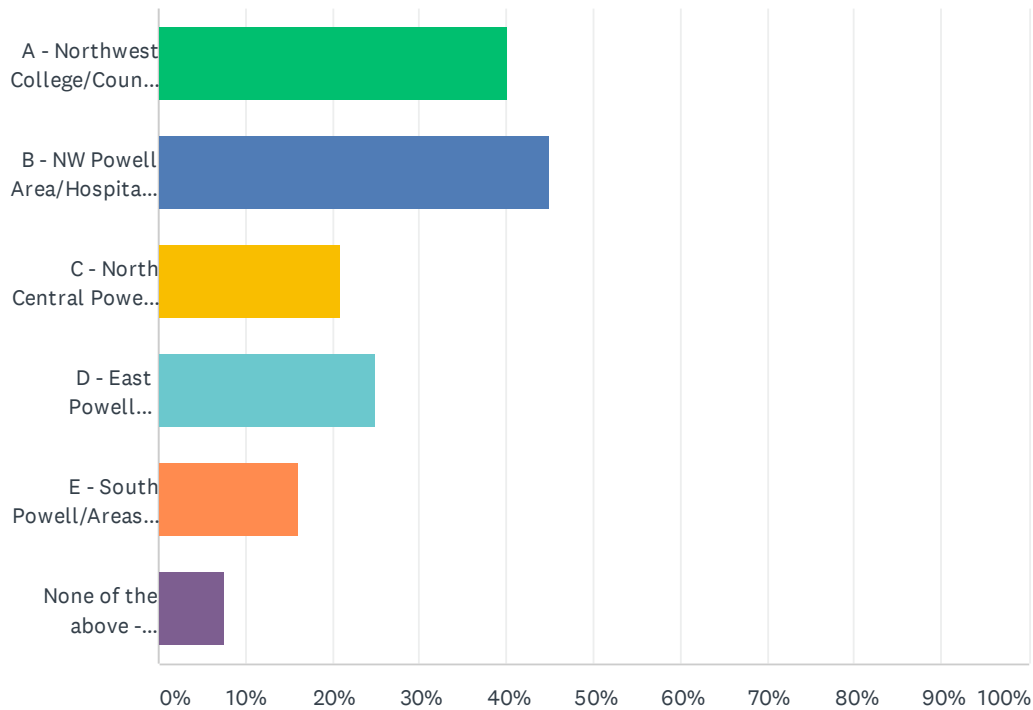
Answered: 252 Skipped: 0



ANSWER CHOICES	RESPONSES	
Never or Rarely	56.75%	143
Once a month	13.10%	33
Once a week	11.11%	28
Several days a week	19.05%	48
TOTAL		252

Q4 In which neighborhood(s) do you most frequently start and/or stop your bike, walking, or running journeys? (see image below)

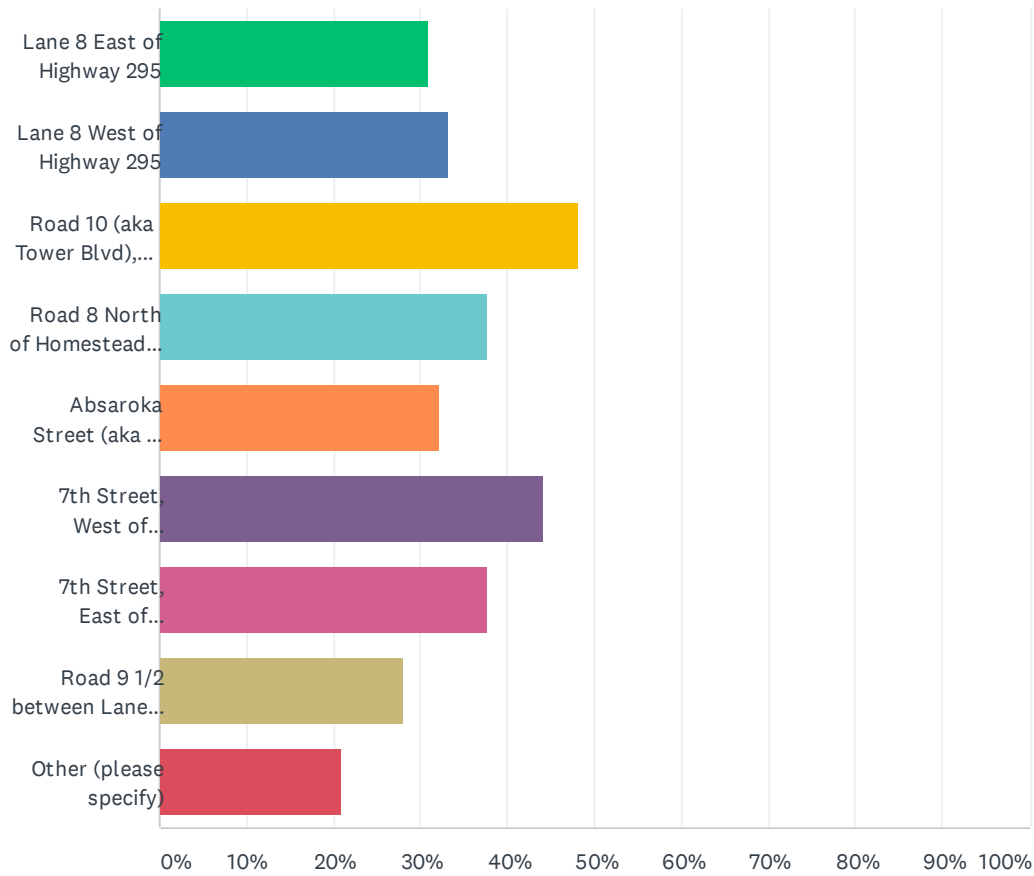
Answered: 249 Skipped: 3



ANSWER CHOICES	RESPONSES	
A - Northwest College/County Annex/Areas around Lane 8 & Road 10/HWY 295	40.16%	100
B - NW Powell Area/Hospital/Westide Elementary	44.98%	112
C - North Central Powell Area/Fairground/Middle School	20.88%	52
D - East Powell Area/High School/ Aquatic Center/Homesteader Park	24.90%	62
E - South Powell/Areas South of the State Highway	16.06%	40
None of the above - everywhere else	7.63%	19
Total Respondents: 249		

Q5 When, walking running, or biking, I often follow the following routes (check all that apply):

Answered: 229 Skipped: 23



ANSWER CHOICES	RESPONSES	
Lane 8 East of Highway 295	31.00%	71
Lane 8 West of Highway 295	33.19%	76
Road 10 (aka Tower Blvd), north of Lane 9	48.03%	110
Road 8 North of Homesteader Park	37.55%	86
Absaroka Street (aka HWY 295), south of Lane 8	32.31%	74
7th Street, West of Absaroka Street	44.10%	101
7th Street, East of Absaroka Street	37.55%	86
Road 9 1/2 between Lane 8 and W 7th Street	27.95%	64
Other (please specify)	20.96%	48
Total Respondents: 229		

#	OTHER (PLEASE SPECIFY)	DATE
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1	Rd 9 to In 14 to wildwood dam	6/10/2021 12:03 PM
2	Near college to Lane 7	6/3/2021 7:20 AM
3	Bighorn NRA, Beck Lake, North Fork	4/22/2021 3:13 PM
4	None	4/18/2021 3:13 PM
5	Ave K. 6th st.	4/13/2021 1:41 PM
6	Park, 1st st, N. Ferris, 5th st	3/22/2021 9:38 PM
7	Fifth street to homesteader	2/28/2021 2:18 PM
8	Monroe and Madison between CrR8 and Ingalls	2/27/2021 2:36 PM
9	South on 295	2/26/2021 11:53 AM
10	HWY 295 to Polecat Bench	2/26/2021 9:53 AM
11	Monroe Street, Road 8 1/2	2/10/2021 10:07 AM
12	In area B we frequently travel the dirt road between the homes and agricultural field.	2/9/2021 8:42 AM
13	Farm road off 7th to Rogers Meat Processing	2/6/2021 1:06 PM
14	west on Lane 9	2/5/2021 2:17 PM
15	None	2/5/2021 9:06 AM
16	Didn't know there were routes	2/5/2021 8:27 AM
17	Only Homesteader Park	2/5/2021 7:02 AM
18	Rd 8H south side	2/4/2021 10:54 PM
19	I own a car	2/4/2021 9:55 PM
20	Lane 9	2/4/2021 9:54 PM
21	Not sure	2/4/2021 9:41 PM
22	Homesteader path since that is the only place I feel safe.	2/4/2021 9:30 PM
23	Ave E, 2nd St, Grand St, Mountain View St	2/4/2021 7:15 PM
24	Biking is on Lane 13 West many of the roads and dirt roads south of the tracks. Running is on the dirt road that follows the canals off of lane 13 past the willwood irrigation center	2/4/2021 5:20 PM
25	Road 8 1/2 South of city limits and between Lane 11	2/4/2021 4:47 PM
26	Avenue e, lane 9	2/4/2021 4:41 PM
27	B & C	2/4/2021 2:55 PM
28	Lane 10/ lane 11 north of the state highway	2/4/2021 1:41 PM
29	As mentioned previously	2/4/2021 12:41 PM
30	Jones Street and E Monroe	2/4/2021 12:39 PM
31	Walk or ride bike downtown and in various neighborhoods mostly west side of Absaroka and sometimes ride or walk downtown.	2/4/2021 11:33 AM
32	South side, Monroe, Road 8	2/4/2021 11:22 AM
33	Coulter Ave (Powell/Cody hiway)	2/4/2021 9:27 AM
34	Division	2/4/2021 7:36 AM
35	Cross Highway to Legion baseball fields, bent street	2/1/2021 5:37 PM
36	Neighborhood around my home	2/1/2021 3:14 PM
37	I use all roadways and canal roads available	2/1/2021 2:39 PM

38	7th street	1/31/2021 4:25 PM
39	Division	1/31/2021 2:36 PM
40	Field north of NWC	1/31/2021 1:56 PM
41	I live near Cody and walk there but o support a path for Powell folks!	1/31/2021 9:10 AM
42	None	1/26/2021 1:12 PM
43	Ave E to ln 9, Greenfield dr, tower boulevard	1/26/2021 8:10 AM
44	Road 8 south of homesteaders park.	1/25/2021 10:22 PM
45	Road 7 by cemetery	1/25/2021 7:44 PM
46	Homesteader park for safety, but live in the tower road area.	1/25/2021 7:19 PM
47	The path along Hwy 14A in front of the car wash, etc.	1/25/2021 6:33 PM
48	I bike and run all over the Powell area, but mostly stay on the western side of town and out of town on Ave E, Road 10, Road 11, Lane 8.	1/25/2021 5:50 PM

Q6 When walking, running or riding a bike in the north Powell area, what specific safety concerns do you have? What specific locations or intersections do you think are unsafe? Also, please list any other comments, questions, or concerns.

Answered: 156 Skipped: 96

#	RESPONSES	DATE
1	To close to cars that don't watch out for walkers/bikers. The nicer the path. The more it will be used.	6/12/2021 9:35 AM
2	Busy	6/10/2021 12:03 PM
3	Gravel isn't good for kids so paved	6/10/2021 11:52 AM
4	Lack of dedicated BIKE LANES! Thank you for asking.	6/6/2021 9:02 PM
5	People speeding in neighborhoods, no sidewalk.	5/20/2021 1:16 PM
6	Gravel isn't my favorite for running or biking. I like that it's safer than the road, but I prefer pavement.	5/10/2021 8:02 PM
7	Lane 8 and tower blvd	5/8/2021 9:39 AM
8	Trucks not respecting bikers or walkers.	5/7/2021 10:31 AM
9	Often cars do not make room for pedestrians or cyclists and pets, and the gravel/sand from the snow control during the winter is still unclean, making it near impossible to safely stop suddenly at intersections	5/3/2021 7:24 PM
10	There's not enough room, hard to ride a bike on gravel. It would be nice to have a 5k route with a paved surface	4/28/2021 4:12 PM
11	Getting hit by car, no bike path, no room, riding on rocks, dog attacks.... most are unsafe/unfit for bikes, designed for cars only. Need more Actual paths desperately with paved surface, not dirt or gravel	4/28/2021 4:12 PM
12	Need a bike lane	4/25/2021 7:23 PM
13	Never go there, lacks environmental diversity.	4/22/2021 3:13 PM
14	Traffic is almost always respectful of pedestrians.	4/13/2021 1:41 PM
15	Traffic!! I worry about running/biking with all the traffic and the speed with which they are traveling	4/11/2021 4:52 PM
16	For exercise I mostly ride N of Lane 8. Very little traffic and generally drivers are very polite and give me lots of space. Same situation in more built up areas but I ride more aware of vehicles and give them space too.	3/27/2021 2:30 PM
17	Not enough room for traffic and walking, especially with dogs.	3/23/2021 9:04 PM
18	Not enough room for traffic and walking, especially with dogs.	3/23/2021 7:20 PM
19	Mostly finding smooth running surfaces, Sidewalks often choppy, running on side of street easier on knees and ankles.	3/22/2021 9:38 PM
20	Traffic, people rarely give room	3/17/2021 12:33 PM
21	Not run the entire distance traffic is fast and routes aren't marked	3/15/2021 4:06 PM
22	When riding Airport Hill on 295, I'm concerned about vehicles coming too close and not offering a warning honk	3/14/2021 11:14 AM

23	Specific bike paths/lanes. Lights	3/7/2021 6:22 PM
24	Cars not paying attention	3/7/2021 2:46 PM
25	Narrow streets/ no bike Lanes even on highway	3/5/2021 11:01 AM
26	Traffic, lack of space for running, speed limits.	3/5/2021 10:52 AM
27	Lane 8, west of Rd 9 to Rd 10 and R 10 south to Lane 8 are dangerous, particularly when the conditions are muddy or snow packed. Lane 8 between NWC and Road 10 has several blocks where there is no sidewalk for pedestrian travel. Also several owners along this route do no clear snow and ice from their Lane 8 sidewalks.	3/4/2021 3:58 PM
28	No consistent sidewalks to speak of along road 10. Mid to south portions of Division street sidewalks are in terrible shape. Avenue E has great sidewalks and it would be nice to see that follow out to road 10 and around to 7th street and back along division to avenue e.	3/4/2021 6:12 AM
29	walk on road 8 and 7th street early in the morning - worry about traffic and ice in the winter time	3/1/2021 2:25 PM
30	Sidewalk/path not existent on all streets specifically Cheyenne east of the college and that road floods/ices over and the other place on our route is lack of sidewalk or path on RD 8 going west of the annex	2/28/2021 2:18 PM
31	I do not have any concerns	2/26/2021 7:05 PM
32	Traffic, lack of a shoulder	2/26/2021 11:53 AM
33	It would be great to have a designated bike land that goes from Absaroka north to the Annex and then up to the polecat bench.	2/26/2021 9:53 AM
34	Wish there was a bike lane going up to airport (heading north on Absaroka). Would be a great place for hill work but way too dangerous with big trucks. Pretty sure someone on bike was killed there)	2/22/2021 12:50 PM
35	Walking on lanes and roads are dangerous because traffic never follows posted speed limits and there are no shoulders on the road. But in town there is no good path and dogs are fence aggressive. The college put up their signs about dogs, effectively halting extra walking paths for people and excluding the community. The current gravel path isn't maintained.	2/21/2021 4:24 PM
36	DOGS!	2/18/2021 11:42 AM
37	Riding or walking with traffic and extremely close proximity.	2/18/2021 7:38 AM
38	The city sanitation crew places a dumpster in the middle of the sidewalk near 8th and gilbert, causing pedestrians, often with children, to walk in gilbert st.	2/18/2021 7:38 AM
39	No bike lanes	2/17/2021 1:46 PM
40	I would love to have a designated space to walk, bike, run, and roller blade. I wouldn't be a stressed having my daughter out on the paths with me if it was not also a road. It's currently hard for me to relax much because I'm always stressed about sharing the road with vehicles.	2/15/2021 6:06 PM
41	We need a path off the road because on 295 on my way to work everyone wears black and they are hard to see and they are on the road	2/15/2021 6:05 PM
42	There are no sidewalks in some places. A confusion with 4-way and 2-way stops, no shoulders when riding.	2/12/2021 7:26 PM
43	We farm adjacent to most of the locations mentioned! The bike path you have already created is seldom used... people walk right down the highway. It is a huge safety issue and promoting more traffic is not a responsible idea. Also, we farm or own a large portion of the land bordering your proposed bike path. What measures are being taken to keep walkers, and bikers off of private land? Mornings and evenings are especially dangerous as people are one the roadway even when the bike path is right beside it. Promoting more people to bike and walk along a designated truck route is not a safe or sound idea!	2/12/2021 12:08 PM
44	Avenue G and Mountain View stop sign gets ran often and people are often driving above the speed limit on both of these streets.	2/11/2021 5:46 PM
45	On many Powell blocks sidewalks end, often forcing a person to cross a busy street they	2/9/2021 8:42 AM

wouldn't normally have to. Sidewalks in general could use repair. The most dangerous section is crossing the Highway at any location. The crosswalk to the JR. High is the only safe area to cross that road on foot or on bike. We also still have intersections in Powell proper that have no right-of-way (no stop sign or yield signs). The Wells Fargo bank intersection also continues to be a serious hazard. The ridiculous jumble at (I think it is called Powell Electric) where W. South St and S. Fair St link up twice is also very unsafe. Good luck with your project!

46	walking from the intersection of road 8 west on lane 8 to road 10 south to 7th st traffic is dangerous need PAVED PATH	2/8/2021 2:58 PM
47	I walk the bike path 5 days a week or more. My biggest frustration is that there's a house on the bike path with a dog -- and no fence -- that barks and runs right up to us. A golden retriever. I have a blind dog I walk, and she's afraid of other dogs who run up to her. And, I have a large mastiff, who's protective. It makes me angry that this dog is out barking in a non-fenced yard pretty much every day for more than a year.	2/6/2021 10:29 PM
48	The current bike path needs to be paved. It's no fun riding thru a gravel pit. There needs to be a designated path where the walkers, bikes, and runners are safe from vehicles on the streets and roads.	2/6/2021 2:55 PM
49	Being a newbie to the area I originally walked from my house on 7th St to Lane 8 going through the farm roads. From Lane 8 I would head back to 7th St. The path was a big square on the map. I found the traffic on Lane 8 a bit scary due to semi trucks and others exceeding the posted speed limit. So to stay safe, I modified my walking path to stay on a farm road only. It keeps me closer to my home and I'm never bothered by traffic. I often see other people there walking there dogs. The farm roads are not posted so I'm hoping that I'm not breaking a trespass law. Am I good to go?	2/6/2021 1:06 PM
50	I often walk the alleys as they are level and relatively free of traffic. I don't like riding bikes with traffic and I never ride on the sidewalks.	2/6/2021 12:38 PM
51	Gravel paths are terrible and I will continue to walk on the road! I don't the existing gravel paths! Too deep gravel	2/6/2021 10:42 AM
52	Getting hit by a car on road 10.	2/6/2021 8:52 AM
53	Not having an adequate shoulder to run/ride on. Sidewalks are not always even and some areas don't have sidewalks.	2/5/2021 7:28 PM
54	The current route isn't payved so riding a road bike is out of the question. I very rarely run it because it usually cuts distance off my route. The intersections that are the most difficult in my opinon is crossing the Powell/Cody Hwy from Tower Blvd to continue south on Road 10. Really any intersections crossing the Powell/Cody hwy to head south are difficult and dangerous to cross. I'd like to see a defined bike path heading West on Ave E to badger basin and then badger basing heading both north and south.	2/5/2021 7:15 PM
55	Absoraka north of 7th to the Annex because of fast traffic. Near Shopko to road 11 the cars drive very fast. Crossing Division. Third and second streets the cars don't stop between Division and Absoraka.	2/5/2021 6:42 PM
56	I think of cars, and other things like animals	2/5/2021 4:35 PM
57	I live on Lane 9 west of Road 10. I have seen a heavy increase in traffic in the past few years. I used to walk on Lane 9 and Road 10 but do not feel safe any longer because of the increase in truck and auto traffic. The Lane 8 path to homesteader park is not clearly marked and needs to be paved. Another reason to create paths is to attract and keep our young people in the community. When young people look for a community to settle in, biking, walking paths are an important attribute.	2/5/2021 2:17 PM
58	I don't really feel I have a major safety concern. I exercise in other cities when I travel for work and I think running or biking on our roads/shoulders is not a major safety concern but could be better. I think we should really investigate a partnership with the Irrigation Districts for use of their canal roads for running/biking. There are miles and miles of irrigation roads that make great exercise paths.	2/5/2021 12:18 PM
59	Corner of 7th st and Road 10 can be a safety issue. Road 10 has a lot of early morning foot traffic, visibility is difficult with traffic moving in both directions and runners/walkers occupying space as well. Ave E Lane 9 has a nice wide shoulder which is why I prefer to run that direction. It would be nice if Powell and the surrounding area on the outskirts could offer a	2/5/2021 11:56 AM

better trail system for runners/walkers and bikers. The canal roads are great for runners and walkers but weather plays a significant role in whether they can be used or not.

60	Traffic above speed limit and failure to yield any road space	2/5/2021 11:18 AM
61	Having to share road with vehicles is risky. The shoulders are often narrow. To get off the road into the ditch is difficult as the ground is not level. There is no signage to warn vehicles about bikers/runners/walkers. It's also important to keep in mind the dangers of NOT getting outside to move/exercise. We, as a community, owe it to the health and well-being of our neighbors to provide safety for outdoor recreation AND to encourage it.	2/5/2021 11:16 AM
62	Unleashed dogs and dog waste. There are areas we avoid now due to the number of dogs and the amount of waste.	2/5/2021 9:38 AM
63	Absoroka and N Beckman area;	2/5/2021 8:27 AM
64	Sidewalks are in unsafe condition in older Powell areas, ie: avenues A,B,C,ect. These need to be addressed. When windy conditions persist it is easier to walk in town where there is protection from wind. Potential of falling on these unsafe sidewalks force some people to elect to not walk at all, or choose paths that may put them in harms way mingling with vehicle traffic on roads and lanes near town. Happy to see that this issue is getting addressed, lets just spend our dollars wisely, and benefit to all.	2/5/2021 7:38 AM
65	None. Use your head!!	2/5/2021 6:06 AM
66	None	2/5/2021 12:26 AM
67	West side	2/4/2021 11:49 PM
68	All of division st	2/4/2021 10:57 PM
69	High school area is dangerous for kids to get to and from, no clear crossings and many kids slowly walk across the streets to their cars or homes slow and without even looking. There isn't a distinct walking path for students. With people walking, bikes, cars, and busses going it makes for a lot of "traffic" The kids not using the crosswalk from the middle school to get to the library has been bad a few times for me as well. I've had kids just jump out in front of me before. That intersection gets crazy too but at least there is a controlled 4 way stop. If the kids would just use it daily and not run across the middle of the street between moving vehicles. Scared someone will get hurt one day.	2/4/2021 10:54 PM
70	I used to walk a five mile square 7th St past college to intersection with Lane/Road 10 by Trapper West soccer fields, turn right, take next right past Roger's Meats, past County Annex, behind HS, turn right walk past side of HS. I was nearly hit several times by people driving way too fast or on their phone along those narrow county roads with no shoulder. The only place along there I felt safe is the gravel path area. I quit because it was too dangerous. My daughter runs along Lane 8 a lot and it scares me!!	2/4/2021 9:30 PM
71	I take walks daily or weekly (depending on the weather) with my 1 1/2 year old (pushing him in a three wheeled stroller). The park next to the middle school and south of the fair grounds only has three sides that have a sidewalk and the sidewalk is deteriorated to the point I walk on the street. While walking with my son I have also noticed several areas in town with no sidewalk such as the area between the highschool and the preschool on 7th Street (north side of the road) The entire fairgrounds has no sidewalk around it's perimeter either. Same with the North and East sides of the middle school track/football field. I have walked north on Gilbert and East on lane 8 then down to the high school. There are no sidewalks at all on lane 8 or on the west side of road 8(except once you get to the high school), which is understandable, since it is next to a farm feild. Homesteader park has areas like the entrance next to the Armory and dog park that have no sidewalks either. I like walking over to NWC, but there is no complete sidewalk on 7th Street to the west of Absorka on the south side of the appartments.	2/4/2021 9:24 PM
72	7th street from PHS/Road 8 all the way to Road 10/Tower Blvd.	2/4/2021 8:58 PM
73	It is hard to walk or bike on the gravel along Lane 8 and Tower Blvd making it necessary to go on the road.	2/4/2021 8:32 PM
74	I worry about cross walks and sidewalks. The sidewalks are often blocked off or have cars half in half out. It makes it hard to take kids on bike rides without worrying about them. The existing bike path always has goat heads and pops the tires of our bikes.	2/4/2021 8:19 PM

75	The biggest safety issue is lack of separation from traffic. A completely separate path is nice, but even a useable shoulder is better than nothing. Additionally, separate bike paths are often not built/maintained well enough to ride comfortably. Within town, the abundance of intersections combined with small building setbacks create many visibility issues between cars and pedestrians. Thanks for taking the time to look into this. Good luck!	2/4/2021 7:15 PM
76	Too many vehicles not willing to share road	2/4/2021 6:35 PM
77	Lane 8 traffic and road 295 with no shoulder almost no where to ride a bike	2/4/2021 5:38 PM
78	I do not run or bike north of Powell. A long path would be a wonderful asset to Powell. I would utilize the trails as would many other residents who live south of Powell.	2/4/2021 5:20 PM
79	Are you not considering the south Powell area? Every intersection is unsafe because there are no bike lanes and drivers are largely unaware of pedestrians. Crossing the 4 lanes is the worst even with the new pedestrian crossings.	2/4/2021 4:47 PM
80	Fast trucks, with no lanes specific for bikes on avenue e lane 9	2/4/2021 4:41 PM
81	uneven concrete sidewalks, snowy sidewalks not be shoveled.	2/4/2021 2:55 PM
82	cars speed by, the gravel walkway is difficult	2/4/2021 2:50 PM
83	The highway!!!!	2/4/2021 2:40 PM
84	Airport hwy, 295 , too narrow to ride bike on , I ride west on lane 9 , like to ride on different routes, Just not enough room for cars and bikes, most drivers are good, but some will see how close than get by bike rider without hitting them at 70 mph . 😊	2/4/2021 1:29 PM
85	Not enough cross walks especially by the fairgrounds by now cap building. Need a safe crossing for youth clubs kiddos walking from middle school	2/4/2021 1:27 PM
86	Bad sidewalk areas on Division. Intersection of Avenue E and Division is not a safe crossing area to get to downtown Powell. Also, there are an awful lot of people that are not clearing snow/ice from sidewalks.	2/4/2021 12:41 PM
87	Road 10 especially during beet harvest. I walk with my daughter in a stroller and it does not feel safe. Since I live in trapper village west, I have to walk on road 10 if I want to walk from my apartment to anywhere in town. Lane 8 and Road 10 intersection is not safe mostly due to motorists ignoring the stop sign there.	2/4/2021 12:39 PM
88	Only the north side of town is being considered for bike paths!!	2/4/2021 12:39 PM
89	Lane 8 traffic north of high school.	2/4/2021 12:24 PM
90	Road 10 and lane 8 are dangerous. I'm surprised no one has been hit by a car	2/4/2021 11:40 AM
91	I think we need a specific bike and walking path where we can feel safe for a long distance and not being along a busy highway or road. Somewhere a ways off the traffic areas. Like between Powell and Ralston or Cody or along Ave E, once again not connected to the highway or road. there should be a separation between the road or highway and the path. I know many towns have these paths that go quite a distance. Thanks!	2/4/2021 11:33 AM
92	I am disabled! I'd like wheelchair accessible paths. All my answers in the survey were for where I'd like to travel in my power chair -- IF paths were safe and accessible for me. Please consider the needs of the disabled in your planning.	2/4/2021 11:22 AM
93	lack of sidewalk on 7th street areas between Division and Grand	2/4/2021 10:56 AM
94	In general, traffic is the only safety issue I have while walking or bicycling.	2/4/2021 10:25 AM
95	Traffic	2/4/2021 10:24 AM
96	No pathway on Road 10	2/4/2021 9:27 AM
97	Heading north on Hwy 295, walking on the west side, there's not much separation between the walker and oncoming traffic. Also, crossing Hwy 295 to get to Lane 8 to walk east is a concern.	2/4/2021 8:15 AM
98	Not enough room to move to the side from ongoing traffic. Horrible sidewalks.	2/4/2021 7:36 AM
99	Coulter ave	2/3/2021 3:33 PM

100	Traffic on S Absaroka St. and crossing Coulter Ave. in general.	2/3/2021 2:31 PM
101	There are sections where I walk road surfaces because shoulders are too narrow and overgrown or uneven.	2/3/2021 9:39 AM
102	Oncoming traffic when there is no where to step off the road. Large vehicle are always scary when walking or riding my bike.	2/2/2021 5:06 PM
103	Who ever organized this, know that your awesome. If anyone gives you are hard time about doing this tell them they are being turds. Last thing, every great community has these types of paths.	2/2/2021 4:53 PM
104	Along division street it's fairly difficult to turn immediately if there's heavy traffic	2/2/2021 3:40 PM
105	City streets speed limits. Children crossing the street when vehicles are traveling at 25 mph with vehicles parked along the road.	2/2/2021 2:22 PM
106	In some areas along lane 8 west of NWC there are no sidewalks to walk on.	2/2/2021 12:36 PM
107	Road 10 is hazardous with large trucks and farm equipment running. A walking/bicycle path on this road would not be safe. Traffic is especially bad in the morning from 6 until 8:30 and again in the afternoon after school is out until about 6 in the evening.	2/2/2021 8:18 AM
108	The roads are to narrow for biking or running on the roads safely. The gravel paths are ok for walking. For running the gravel isn't as easy to run on and for road bikes it is impossible.	2/2/2021 6:33 AM
109	Gravel path North of the high school is not finished forcing either walking in the uneven brush or on the highway.	2/1/2021 10:13 PM
110	Would love a path that is easy to walk and ride bikes, gravel is tough.	2/1/2021 5:37 PM
111	Light, I feel like most of the sidewalks are poorly lit.	2/1/2021 5:11 PM
112	The light at Coulter and Bent. The area south of Coulter does not have good (or any) sidewalks and the area leading up to the light is not easily navigated by someone with vision issues.	2/1/2021 3:14 PM
113	Lack of shoulders or marked areas for bike and foot traffic on lane 8 and road 9 I have long felt Piwell needs to improve quality of life with bike and pedestrian areas so we can enjoy the beauty around us without fear of getting run over.	2/1/2021 2:39 PM
114	Lack of traffic separation, forcing walks in the easement away from pavement, on Road 10 north of Lane 9 (Ave. E) and Lane 8 west of Wyo. Hwy. 295. The path on 295 north of Seventh to Lane 8 is wonderful and I appreciate the snow removal.	1/31/2021 5:53 PM
115	Blind spots, blind intersections, too many cars driving too fast (depending on the road).	1/31/2021 5:48 PM
116	Minimal shoulder on lane 8, west of the annex	1/31/2021 5:14 PM
117	Crowding as the roads do not have much room.	1/31/2021 5:07 PM
118	Light	1/31/2021 4:41 PM
119	Getting hit by a car.	1/31/2021 4:25 PM
120	Lack of sidewalk on 7th west of NWC	1/31/2021 2:36 PM
121	Lack of clearly defined path on Rd 10. Lack of lighting on Road 10. Sections of missing sidewalk on 7th Street.	1/31/2021 2:25 PM
122	Other dogs not on leash. Cars driving too fast.	1/31/2021 1:56 PM
123	Cars driving on the road	1/30/2021 5:49 PM
124	Drivers not paying attention	1/30/2021 2:52 PM
125	The gravel pathway along Lane 8 is not a good surface for riding a bike or running, and is poorly defined as a pathway. There is little or no separation from the vehicle traffic.	1/30/2021 9:46 AM
126	The intersection of road 8 and Absaroka seems most unsafe	1/30/2021 9:12 AM
127	I avoid walking on Lane 8 at night- cars go pretty fast and I don't want to get hit	1/29/2021 6:59 PM
128	Not paved path must ride and walk on road vehicle traffic doesn't leave room	1/29/2021 4:35 PM

129	I do not use the gravel pathway usually ride on the pavement. Would prefer future paths be paved.	1/29/2021 2:46 PM
130	Powell needs bike paths that are paved. Riding on gravel/stones is difficult.	1/27/2021 3:07 PM
131	All along lane 8 is unsafe as vehicles are often speeding and there's no shoulder since the canal switches sides.	1/27/2021 12:29 PM
132	Traffic	1/27/2021 9:57 AM
133	I also commute along Lane 8 every weekday morning and am highly concerned about all of those who utilize this area (and other areas) for walking/running/biking as it can create a very dangerous situation. Please provide a safe pathway for our citizens so that those who are on foot are safe and those that are driving do not ever have to face the possibility of a tragedy that they will have to live with. Thank you.	1/27/2021 9:40 AM
134	Won't let kids cross Coulter	1/27/2021 8:08 AM
135	There are so many areas around towns where the sidewalk ends and doesn't pick up on the other side of the street. This is a problem when I want my children to be safe.	1/26/2021 2:33 PM
136	1st did not know there was a bike path. I enjoy walking at homesteader park. Also with the economy the way it is a am not sure if this is the right time to consider a project such as this.	1/26/2021 11:32 AM
137	Drivers dont pay attention. , it's not pedestrian friendly anywhere in powell , not even in cross walks. Drivers will purposely Not make eye contact. And drive even if you are already in the cross walk.	1/26/2021 8:10 AM
138	Narrow streets, rocks flinging at people as cars drive by, no clear walk or bike paths, to much gravel and unlevel areas for people who utilize these areas resulting in twisted ankles or flat tire, concern for lack of highly visible signage especially for youthful drivers to be aware of pedestrians and bicycles	1/26/2021 8:08 AM
139	Vehicles passing n the street while riding bikes with my children.	1/26/2021 7:37 AM
140	Lots of high speed traffic on Land 8 north of town between road 11 - annex and road 8	1/26/2021 6:41 AM
141	Lack of lighting, people not wearing light colors or reflective clothing to be seen after dark or before sun up.	1/26/2021 6:29 AM
142	Intersection of road 8 (Panther Blvd) and 14A (Coulter Ave)	1/25/2021 10:22 PM
143	The existing bike/walking lanes need upkeep. The one along lane 8 is bad the road drops off way to abrupt.	1/25/2021 9:31 PM
144	People driving vehicles do not pay much attention to the safety of people walking or riding	1/25/2021 9:13 PM
145	Downtown area is extremely dangerous, especially near post office where there is angled parking. People rarely look for cyclists. Road up to airport is too dangerous to ride, which is a shame (narrow shoulders)	1/25/2021 8:16 PM
146	Road 10 intersections of Road 8 1/2 and Road 10 and lane 8. There are lots of walkers and runners with lots of traffic. A bike / walking path in Powell would be amazing!	1/25/2021 7:48 PM
147	Any of the county roads that go around Powell, the close ones like lane 8, road 10, panther blvd, etc. I've been running by myself pushing a stroller and almost been hit because people get so close to shoulder and are speeding. Lane 8 is the worst with people flying down it. Not only would getting a bike path help but have officers monitor it.	1/25/2021 7:29 PM
148	Cars and inattentive drivers.	1/25/2021 7:22 PM
149	No safe place to walk safely out of traffic except on the path along Lane 8. Lane 8 and Road 10 are very dangerous—the traffic is too heavy and too fast.	1/25/2021 7:21 PM
150	Road 10 is a definite need. People walk and bike on the road daily. There is plenty of room for a path that can get them off the roadway. I live off of tower road but drive my dogs to homesteader daily to walk safely.	1/25/2021 7:19 PM
151	Road 9 at Lane 8 is very dangerous to cross. Definitely bike/walking lanes are needed on Tower Blvd, Lane 8 and other outside of the city limits. Also, many sidewalks are not maintained during the winter seasons. The walkway by the nursing home on Grand Street are	1/25/2021 7:14 PM

slick even when they are wet in the summer. The concrete is not user friendly at times, hence many people walk in the street. On some sidewalks there are bushes/trees that are overgrown or hanging which prevents a clear path.

152	In general people don't know that pedestrians have the right away.	1/25/2021 7:02 PM
153	No crosswalk lines and vehicles do not stop. When biking you should bike with traffics not against. When running you should run against traffic.	1/25/2021 6:54 PM
154	Drivers aren't aware of pedestrians.	1/25/2021 6:46 PM
155	It's very dangerous to ride on or along the roads at the edges of town. Concerned about safety with out of town travelers so close to the Homesteader trails next to the welcome center.	1/25/2021 6:33 PM
156	Paths need to be paved or they won't be used!	1/25/2021 5:33 PM

Appendix C – Conceptual Cost Estimates

Conceptual Cost Estimate

Alternative 1

Pave existing Lateral C pathway

Description	Units	Qty	Unit Price	Total Price
Mobilization and general conditions	LS	1	\$5,401.20	\$5,401
Traffic control	LS	1	\$2,000.00	\$2,000
Asphalt pathway, 10-ft wide, 2.5-in thick	LF	2,760	\$20.75	\$57,270
Leveling course, crushed base 3-in thick	LF	2,760	\$5.00	\$13,800
Embankment and excavation	CY	500	\$20.00	\$10,000
Signage	EA	5	\$700.00	\$3,500
Striping	LF	2,760	\$1.25	\$3,450
Subtotal				\$95,421
	Easements		LS	\$ -
	Engineering services		20%	\$ 19,084
	Contingency		20%	\$ 19,084
	TOTAL			\$ 133,590

Conceptual Cost Estimate

Alternative 2

Pave existing Lane 8 pathway without separation

(east of Hwy 295)

Description	Units	Qty	Unit Price	Total Price
Mobilization and general conditions	LS	1	\$6,051.00	\$6,051
Traffic control	LS	1	\$2,000.00	\$2,000
Asphalt pathway, 10-ft wide, 2.5-in thick	LF	2,950	\$20.75	\$61,213
Leveling course, crushed base 3-in thick	LF	2,950	\$5.00	\$14,750
Embankment and excavation	CY	500	\$20.00	\$10,000
Approach improvements	EA	1	\$5,000.00	\$5,000
Signage	EA	6	\$700.00	\$4,200
Striping	LF	2,950	\$1.25	\$3,688
Subtotal				\$106,901
		Easements	LS	\$ -
		Engineering services	20%	\$ 21,380
		Contingency	20%	\$ 21,380
		TOTAL		\$ 149,661

Conceptual Cost Estimate

Alternative 3

**Pave existing Lane 8 pathway with gravel separation
(east of Hwy 295)**

Description	Units	Qty	Unit Price	Total Price
Mobilization and general conditions	LS	1	\$8,212.50	\$8,213
Traffic control	LS	1	\$2,000.00	\$2,000
Asphalt pathway, 10-ft wide, 2.5-in thick	LF	2,950	\$20.75	\$61,213
Leveling course, crushed base 3-in thick	LF	2,950	\$5.00	\$14,750
Pit run	CY	875	\$35.00	\$30,625
Embankment and excavation	CY	700	\$20.00	\$14,000
Approach improvements	EA	1	\$5,000.00	\$5,000
Signage	EA	8	\$700.00	\$5,600
Striping	LF	2,950	\$1.25	\$3,688
Subtotal				\$145,088
		Easements	LS	\$ -
		Engineering services	20%	\$ 29,018
		Contingency	20%	\$ 29,018
		TOTAL		\$ 203,123

Conceptual Cost Estimate
Alternative 4
New pathway Lane 8 between Road 10 and Hwy 295

Description	Units	Qty	Unit Price	Total Price
Mobilization and general conditions	LS	1	\$18,993.00	\$18,993
Traffic control	LS	1	\$2,000.00	\$2,000
Paved pathway incl. asphalt, crushed base, and pit run	LF	5,300	\$40.25	\$213,325
Embankment and excavation	CY	1,200	\$20.00	\$24,000
Approach improvements	EA	4	\$5,000.00	\$20,000
Irrigation and drainage ditch reconfiguration	LS	1	\$25,000.00	\$25,000
Fencing reconstruction	LS	1	\$20,000.00	\$20,000
Signage	EA	8	\$700.00	\$5,600
Striping	LF	5,300	\$1.25	\$6,625
Subtotal				\$335,543
		Easements	LS	\$ 40,000
		Engineering services	20%	\$ 67,109
		Contingency	20%	\$ 67,109
		TOTAL		\$ 509,760

Conceptual Cost Estimate

Alternative 5

New pathway Road 10 between Seventh St and Lane 8

Description	Units	Qty	Unit Price	Total Price
Mobilization and general conditions	LS	1	\$13,420.50	\$13,421
Traffic control	LS	1	\$4,000.00	\$4,000
Paved pathway incl. asphalt, crushed base, and pit run	LF	2,650	\$40.25	\$106,663
Embankment and excavation	CY	900	\$20.00	\$18,000
Approach improvements	EA	4	\$5,000.00	\$20,000
Construct new farm approach	EA	3	\$10,000.00	\$30,000
Irrigation and drainage ditch reconfiguration	LS	1	\$30,000.00	\$30,000
Fencing reconstruction	LS	1	\$7,500.00	\$7,500
Signage	EA	6	\$700.00	\$4,200
Striping	LF	2,650	\$1.25	\$3,313
Subtotal				\$237,096
		Easements	LS	\$ 150,000
		Engineering services	20%	\$ 47,419
		Contingency	20%	\$ 47,419
		TOTAL		\$ 481,934

Conceptual Cost Estimate

Alternative 6

New pathway Road 9 1/2 between Seventh St and Lane 8

Description	Units	Qty	Unit Price	Total Price
Mobilization and general conditions	LS	1	\$10,530.00	\$10,530
Traffic control	LS	1	\$2,000.00	\$2,000
Paved pathway incl. asphalt, crushed base, and pit run	LF	2,600	\$40.25	\$104,650
Embankment and excavation	CY	500	\$20.00	\$10,000
Approach improvements	EA	6	\$5,000.00	\$30,000
Irrigation and drainage ditch reconfiguration	LS	1	\$15,000.00	\$15,000
Landscaping reconstruction	LS	1	\$5,000.00	\$5,000
Signage	EA	8	\$700.00	\$5,600
Striping	LF	2,600	\$1.25	\$3,250
Subtotal				\$186,030
		Easements	LS	\$ 50,000
		Engineering services	20%	\$ 37,206
		Contingency	20%	\$ 37,206
		TOTAL		\$ 310,442

Conceptual Cost Estimate

Alternative 7

New pathway Road 8 from HS to Lane 8, and Lane 8 to existing

Description	Units	Qty	Unit Price	Total Price
Mobilization and general conditions	LS	1	\$13,288.50	\$13,289
Traffic control	LS	1	\$5,000.00	\$5,000
Paved pathway incl. asphalt, crushed base, and pit run	LF	3,650	\$40.25	\$146,913
Embankment and excavation	CY	400	\$20.00	\$8,000
Approach improvements	EA	5	\$5,000.00	\$25,000
Irrigation and drainage ditch reconfiguration	LS	1	\$15,000.00	\$15,000
Fencing reconfiguration	LS	1	\$10,000.00	\$10,000
Signage	EA	10	\$700.00	\$7,000
Striping	LF	3,650	\$1.25	\$4,563
Subtotal				\$234,764
		Easements	LS	\$ 50,000
		Engineering services	20%	\$ 46,953
		Contingency	20%	\$ 46,953
		TOTAL		\$ 378,669

Conceptual Cost Estimate
Alternative 8
Pedestrian improvements along Seventh St

Description	Units	Qty	Unit Price	Total Price
Mobilization and general conditions	LS	1	\$16,207.50	\$16,208
Traffic control	LS	1	\$6,000.00	\$6,000
Paved pathway incl. asphalt, crushed base, and pit run	LF	3,900	\$40.25	\$156,975
Embankment and excavation	CY	50	\$20.00	\$1,000
Approach improvements	EA	12	\$4,000.00	\$48,000
Fencing and landscaping reconfiguration	LS	1	\$40,000.00	\$40,000
Signage	EA	12	\$700.00	\$8,400
Striping	LF	7,800	\$1.25	\$9,750
Subtotal				\$286,333
		Easements	LS	\$ 50,000
		Engineering services	20%	\$ 57,267
		Contingency	20%	\$ 57,267
		TOTAL		\$ 450,866

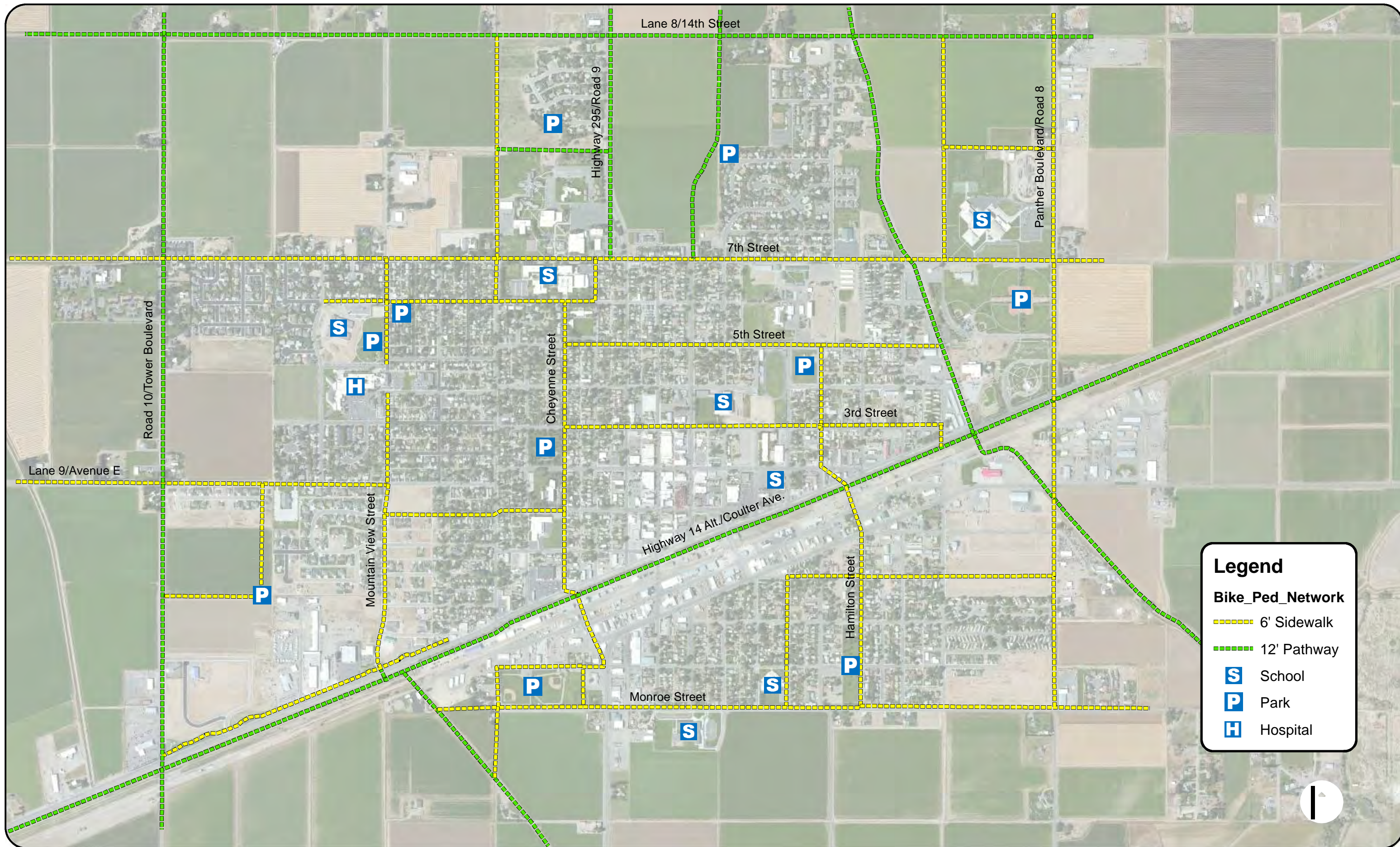
Conceptual Cost Estimate

Alternative 9

Traffic separation along Absarkoka St / Hwy 295

Description	Units	Qty	Unit Price	Total Price
Mobilization and general conditions	LS	1	\$12,838.80	\$12,839
Traffic control	LS	1	\$5,000.00	\$5,000
Paved pathway incl. asphalt, crushed base, and pit run	LF	1,750	\$40.25	\$70,438
Embankment and excavation	CY	50	\$20.00	\$1,000
Approach improvements	EA	7	\$8,000.00	\$56,000
Concrete hardscape	SF	2,320	\$7.75	\$17,980
Landscaping, irrigation, drainage	LS	1	\$50,000.00	\$50,000
Signage	EA	10	\$700.00	\$7,000
Striping	LF	5,250	\$1.25	\$6,563
Subtotal				\$226,819
		Easements	LS	\$ -
		Engineering services	20%	\$ 45,364
		Contingency	20%	\$ 45,364
		TOTAL		\$ 317,546

Appendix D – Figure from 2013 Powell Master Street Plan



Legend

Bike_Ped_Network

- 6' Sidewalk
- 12' Pathway
- S School
- P Park
- H Hospital

POWELL MASTER STREET PLAN
 Bike and Pedestrian Network

Appendix E –Meeting Agendas and Attendees



PARK COUNTY WALKING AND BIKING PATH PLANNING
Scoping meeting with Stakeholders
1:30 PM; September 9, 2020

- 1) Stakeholders invited to the scoping meeting:
 - a) WYDOT
 - b) Park County Engineers
 - c) City of Powell
 - d) Northwest College
 - e) Park County School District #1
 - f) Shoshone Irrigation District
 - g) Larsen's bike shop

- 2) Preliminary Matters
 - a) Lead proponent(s)
 - b) Funding options for potential trails
 - i) Design/construction
 - ii) Maintenance
 - c) Preferred location(s) or locations not desired
 - d) Other

- 3) Trail Purpose(s)
 - a) Transport people from location to location
 - b) Exercise
 - c) Leisure
 - d) Overall Riding/Walking Experience
 - e) Combination of these
 - f) Other -



Areas to connect with trails

- g) Northwest College
- h) Park County Annex
- i) Recreation Center/Homesteader Park
- j) High School
- k) Neighborhoods
- l) Downtown
- m) Other schools
- n) Other parks
- o) Other locations

4) Preferred Alignments/Locations

5) Usage characteristics of trails

- a) Pedestrians
- b) Bikes
- c) Combined
- d) City Code Limitations
- e) Other -



- 6) Trail/Pathway Characteristics – Tied to Purpose (See Page 9-10 of 2013 Master Plan)
 - a) Meandering separated from streets/roads with landscaping
 - b) Adjacent to streets/roads
 - c) Integral to sidewalk
 - d) On roadway or widened roadway
 - e) Right-of-way acquisition on private land
 - f) Width of trails (6' and 12' shown on Powell Master Plan)
 - g) Construction materials
 - i) Gravel
 - ii) Recycled asphalt
 - iii) Chip seal over gravel
 - iv) Hot plant mix asphalt pavement
 - v) Concrete
 - vi) Other -

- 7) Adjacent Traffic/Roadway considerations
 - a) Speed
 - b) Turning movements
 - c) Sight distance
 - d) Clearance
 - e) Signage – Share the Road, etc.
 - f) Intersection blockouts
 - g) Striping
 - h) Other -

PARK COUNTY
 TAP 2020 NORTH POWELL PEDESTRIAN AND BICYCLE FACILITIES PLAN
 JOB NO: 20038.00

SEPTEMBER 9, 2020
 1:30 AM
 CITY OF POWELL CITY HALL

**SCOPING MEETING
 SIGN IN SHEET - PLEASE PRINT**

NAME	COMPANY	PHONE NUMBER	EMAIL
1. Ben McDOWAN	PARK COUNTY PUBLIC WORKS	527-8520	bmcdonald@parkcounty.us
2. BRIAN EDWARDS	PARK COUNTY PUBLIC WORKS	527-8520	bedwards@parkcounty.us
3. Lloyd Thiel	Park County	272-9372	LThiel@parkcounty.us
4. Tim MILLER	CITY OF POWELL PARKS	254 0270	tmiller@cityofpowell.com
5. Andy Metzler	City of Powell Streets	202-0409	ametzler@cityofpowell.com
6. Rob McCray	Park County School Dist 1	351-3605	jmccray@psd1.org
7. Todd Frost	WTRD	587-2220	Todd.Frost@WTRD.GOV
8. JEN HUBBARD	CITY OF POWELL	254 0256	jhubbard@cityofpowell.com
9. Dennis Guilken	NWC	202-1123	dennis.guilken@nwc.edu
10. TRAVIS COUKLIN	EA		
11. ERIK WACHOB	EA		
12.			
13.			
14.			
15.			
16.			
17.			

Park County Board of County Commissioners
Regular Meeting
 Tuesday, October 19, 2021

Chairman Livingston called to order a regular meeting of the Board of County Commissioners of Park County, Wyoming, on Tuesday, October 19, 2021. Present were Vice Chairman, Dossie Overfield, Commissioners, Scott Mangold, Lloyd Thiel, Joe Tilden and First Deputy Clerk Hans Odde.

Commissioner Mangold led the audience in the Pledge of Allegiance.

Executive Session: pursuant to W.S. § 16-4-405 (a) (ii)

Commissioner Overfield made a motion to enter into executive session. Commissioner Mangold seconded and the motion carried.

Commissioner Overfield made a motion to exit from executive session. Commissioner Thiel seconded and the motion carried. No decisions were made in this executive session.

Commissioner Overfield made a motion to act on the item as discussed in executive session. Commissioner Thiel seconded and the motion carried.

Consent Items:

Approve-Minutes, Payroll and Vouchers

Commissioner Thiel made a motion to approve the following consent agenda items; Minutes: 10/12/21, Payroll, Vouchers, Monthly Earnings & Collections Statements from: Clerk, Clerk of District Court & Sheriff; Approve and Allow the Chairman to Sign Qtr. 4 FFY 21 Temporary Assistance to Needy Families/Community Partnership Initiative (TANF/CPI) Report & Vouchers. Commissioner Overfield seconded and the motion carried.

Payee	Amount
360 OFFICE SOLUTIONS	\$698.73
A & I DISTRIBUTORS	\$2,202.23
ACKER ELECTRIC, INC.	\$413.50
ARCSEARCH CORPORATION	\$18,596.00
AT&T MOBILITY	\$445.03
ATCO INTERNATIONAL	\$676.58
B & B FLEET SERVICE	\$144.00
BAILEY ENTERPRISES, INC.	\$219.02
BAILEY ENTERPRISES, INC.	\$80.26
BAILEY ENTERPRISES, INC.	\$6,315.07
BEAR CO., INC.	\$1,834.71
BIG HORN CO-OP MARKETING ASSN.	\$2,104.61
BIG HORN COUNTY CORONER	\$1,500.00
BIG HORN RADIO NETWORK	\$1,778.88
BIG HORN WHOLESALE, INC.	\$1,958.58
BIG VALLEY BEARING & SUPPLY, INC.	\$83.03
BLACK HILLS ENERGY	\$1,099.48
BLATT, TIMOTHY J.	\$400.00
BLOEDORN LUMBER - CODY	\$29.97
BLOEDORN LUMBER - POWELL	\$29.48
BOB BARKER COMPANY	\$1,701.00
BOBCAT OF THE BIG HORN BASIN, INC.	\$726.49
BUILDERS FIRST SOURCE	\$41.94
CARING FOR POWELL ANIMALS	\$750.00
CAROLINA SOFTWARE	\$700.00
CARQUEST AUTO PARTS - #064250	\$587.66
CDW GOVERNMENT, INC.	\$52.11
CENTURYLINK BUSINES SERVICES	\$18.09
CHARTER COMMUNICATIONS	\$303.65
CODY ACE HARDWARE	\$706.04
CODY COUNCIL ON AGING	\$11,750.00
CODY ENTERPRISE	\$174.81
CODY REGIONAL HEALTH	\$110.00
CODY WINNELSON CO.	\$42.21
CODY, CITY OF	\$26,301.22
CORRECTIONAL HEALTHCARE COMPANIES, INC.	\$21,618.97
COUNTY CLERKS ASSOCIATION OF WYOMING	\$300.00

CRISIS INTERVENTION SERVICES	\$2,500.00
CROFT, KIMBALL	\$26.88
CRUM ELECTRIC SUPPLY CO.	\$37.19
CUSTOM DELIVERY SERVICE, INC.	\$186.22
DALLMAN, KIERSTIN	\$60.00
DE HAAN ELECTRONICS	\$2,140.73
DENNEY, EMILY	\$70.77
DENNY MENHOLT CHEVROLET BUICK GMC	\$146.40
DUSTBUSTERS, INC.	\$88,405.10
EICHELE, LARRY	\$26.88
FASTENAL	\$1,057.60
FLOYD'S TRUCK CENTER	\$21.90
FORWARD CODY WYOMING, INC.	\$3,500.00
GALLS, LLC	\$850.10
GARLAND LIGHT & POWER CO.	\$905.85
GOODYEAR PRINTING	\$280.75
GRAINGER	\$752.69
GUERTIN, ANDREW	\$43.83
HATFIELD, JACK R. II	\$26.88
JOHN DEERE FINANCIAL	\$50.16
K-B COMMERCIAL PRODUCTS	\$3,155.56
KEELE SANITATION	\$3,218.82
KENCO SECURITY AND TECHNOLOGY	\$559.55
KINCHELOE PLUMBING & HEATING	\$245.00
KUCK, PAULA	\$60.00
LOMCO, INC.	\$9,945.00
MAJESTIC LUBE, INC.	\$70.20
MANNAHOUSE FOOD PANTRY	\$2,000.00
McINTOSH OIL, INC.	\$4,622.98
MEETEETSE SENIOR CENTER	\$2,500.00
MEETEETSE VISITOR CENTER	\$1,500.00
MEETEETSE, TOWN OF	\$249.20
METTLER-TOLEDO, LLC.	\$3,384.49
MICKELSON, JOSEPH D	\$291.20
MONTANA-DAKOTA UTILITIES CO.	\$762.63
MORRIS, SANDIE	\$29.90
MOTOR PARTS, INC.	\$1,876.45
MURDOCH OIL, INC.	\$28,252.88
MURDOCH'S RANCH & HOME POWELL	\$172.71
NEWMAN TRAFFIC SIGNS, INC	\$11,079.08
NORCO, INC.	\$49.20
OFFICE SHOP, THE	\$844.91
OFFICIAL COURT RECORD	\$1,196.00
O'REILLY AUTO PARTS - POWELL	\$245.96
PARK CO COURT SUPERVISED TREATMENT PROG	\$3,000.00
PARK COUNTY 4-H COUNCIL	\$1,500.00
PARK COUNTY ANIMAL SHELTER	\$2,250.00
PENWELL, STEVE	\$37.12
POWELL ACE HARDWARE, LLC	\$19.98
POWELL ECONOMIC PARTNERSHIP, INC.	\$3,000.00
POWELL SENIOR CITIZENS AGO-GO, INC	\$11,750.00
POWELL TRIBUNE, INC.	\$1,034.96
POWELL VALLEY RECYCLING	\$625.00
POWELL, CITY OF	\$5,944.57
POWER, TIM	\$18.47
PRODUCTION MACHINE COMPANY	\$5,320.19
QED ENVIRONMENTAL SYSTEMS	\$4,980.35
QUALITY PROPANE SERVICE, INC.	\$24.50
RIDGELINE AUTO REPAIR	\$698.81
ROCKY MOUNTAIN INFO NETWORK	\$25.00
ROCKY MOUNTAIN POWER	\$525.15
ROCKY MOUNTAIN WASH, LLC	\$72.00
S&P BRAKE SUPPLY AND DRIVELINE SERVICES	\$312.64
SABER PEST CONTROL	\$120.00
SANOFI PASTEUR, INC	\$16,862.08
SCHLATTMANN LAW OFFICE	\$400.00
SELBYS, INC.	\$89.92
SHERIDAN CO. CLERK OF CIRCUIT COURT	\$2.00
SNOW CREST CHEMICALS	\$579.00
SOLID WASTE ASSOC. OF NORTH AMERICA	\$223.00
STREET, DARCY	\$27.96
SUMMERS, JOSH	\$36.12

SYSKO FOOD SERVICES OF MONTANA	\$1,659.44
TIRE-RAMA BLGS INTERSTATE	\$11,429.00
T-O ENGINEERS	\$722.80
TRACTOR & EQUIPMENT CO	\$1,315.46
TRIPLE L SALES	\$446.87
TYLER BUSINESS FORMS	\$920.79
VISA - #0037 - B. POLEY	\$168.46
VISA - #0045 - B. SKORIC	\$154.00
VISA - #0052 - P. MEYER	\$1,168.88
VISA - #0078 - L. LIVINGSTON	\$623.32
VISA - #0086 - W.E. CRAMPTON	\$382.91
VISA - #0235 - B. EDWARDS	\$2,828.38
VISA - #0250 - C. RENNER	\$2,823.00
VISA - #0359 - S. STEWARD	\$72.48
VISA - #0367 - J. HILL	\$354.34
VISA - #3972 - J. TATUM	\$210.74
VISA - #4079 - S. STEWARD	\$1,898.45
VISA - #4087 - M. CONNERS	\$958.96
VISA - #4111 - S. STEWARD	\$1,262.78
VISA - #5258 - M. GARZA	\$295.81
VISION WEST, INC.	\$1,649.00
WEBSTER, II, C.E., ATTORNEY LLC	\$2,640.00
WHITLOCK MOTORS	\$779.72
WILLIAMS & SONS, LLC	\$180.00
WPCI CONSULTANTS, INC.	\$117.00
WYOMING BEHAVIORAL INSTITUTE	\$5,278.00
WYOMING DEPARTMENT OF EMPLOYMENT	\$150.24
WYOMING DEPARTMENT OF HEALTH	\$255.68
WYOMING DEPARTMENT OF HEALTH/PBL HLTH LB	\$24.00
WYOMING DEPT OF TRANS - MOTOR VEHICLES	\$5.00
WYOMING LAW ENFORCEMENT ACADEMY	\$3,184.80
WYOMING SECRETARY OF STATE	\$60.00
WYOMING STATE PUBLIC DEFENDER	\$400.00
WYOMING WATER WORKS	\$298.00
YATES, LORI	\$60.00
YELLOWSTONE BEHAVIORAL HEALTH	\$10,000.00
YOUTH CLUBS OF PARK COUNTY	\$2,250.00
TOTAL	<u>\$400,402.13</u>

Park County Planning & Zoning Planner II, Kim Dillivan RE: Public Hearing: Amended Plat Sage Creek Subdivision Lot 1

Chairman Livingston opened a public hearing and Mr. Dillivan presented the amended plat for applicants Ryan and Katie Roemmich and their Sage Creek Subdivision of Lot 1 which reconfigures Lot 1 to create lot 101 (20.33 acres) by way of a boundary line adjustment between Lot 1 and an un-platted portion of property immediately to the west. This action will result in the vacation of Lot 1 (10.03 acres). Lot 101 will continue to be for residential use. All agency referrals have been met and no public comment has been received. The Planning Director recommends approval of the partial vacation and amended plat with the conditions listed in the resolution.

Commissioner Tilden made a motion to close the public hearing and Commissioner Thiel seconded. Motion carried.

Commissioner Tilden made a motion to approve the Amended Plat Sage Creek Subdivision Lot 1. Commissioner Thiel seconded and the motion carried. **Resolution 2021-72**

Park County Planning & Zoning Planner II, Kim Dillivan RE: Review Subdivision Improvements Agreement: Pheasant Ridge Minor Subdivision and Final Plat Pheasant Ridge Minor Subdivision

Mr. Dillivan presented the final plat for a five-lot subdivision consisting of four 3.36-acre lots and one 15.41-acre lot for residential use. Approval of this subdivision will create a remainder parcel of 43.61 acres. The applicant, Seaton Smith also requests Board approval of the Subdivision Improvements Agreement that addresses extending electric utilities to proposed Lots 3 and 4. All agency referrals have been met and no public comment has been received. The Planning Staff recommends approval of the final plat and Subdivision Improvements Agreement and that each one be acted upon with a separate motion.

Commissioner Mangold made a motion to approve the Subdivision Improvements Agreement as presented. Commissioner Overfield seconded and the motion carried.

Commissioner Mangold made a motion to approve the Final Plat as presented. Commissioner Overfield seconded and the motion carried.

After discussion about the proposed resolution referencing both the Subdivision Improvements Agreement and the Final Plat Commissioner Mangold withdrew his motion and the second concurred. Motion withdrawn.

Commissioner Mangold made a motion to approve the resolution that includes the Subdivision Improvements Agreement and the Final Plat for the Pheasant Ridge Minor Subdivision. Commissioner Overfield seconded and the motion carried. **Resolution 2021-73**

Park County Planning & Zoning Planner II, Kim Dillivan Re: Public Hearing: Riverside Minor Subdivision-60 Sketch Plan Review

Chairman Livingston opened a public hearing for the Riverside Minor Subdivision and Mr. Dillivan presented the request from Arnold & Vicki Mollett to approve a five-lot subdivision consisting of five 4.0-acre lots, all for residential use. Creation of this subdivision in the GR-P zone will result in a remainder parcel of 45.62 acres. Agency referrals have been met, no public comments have been received and staff recommends approval with conditions.

Commissioner Overfield made a motion to close the public hearing and Commissioner Tilden seconded. Motion carried.

Commissioner Thiel made a motion to approve the Riverside Minor Subdivision-60 Sketch Plan subject to the listed conditions and adding the Right to Farm Act language to the final plat. Commissioner Overfield seconded and the motion carried. **Resolution 2021-74**

Park County Commissioners Re: American Rescue Funds (ARF) – Meeteetse Water Project

Mayor Bill Yetter and Town Clerk Angela Johnson answered questions from the Commissioners regarding their request for \$645,000 of funding from ARF to complete the town's water project. The Commissioners emphasized that the funding cannot be used as matching funds for the project however ARF funds may be used to purchase specific items such as pipe or other physical materials. In further discussion the Commissioners wanted to know if the Town could get by with \$495,000 due to the number of other projects being considered. Mayor Yetter and Ms. Johnson indicated that they understood the restrictions and felt that the project would still go forward with the reduced figure.

Commissioner Thiel made a motion to approve the request for American Rescue Funds from the Town of Meeteetse up to \$500,000 for the purchase of tangible materials. Commissioner Tilden seconded and the motion carried.

Park County Engineer, Brian Edwards and Travis Conklin of Engineering Associates Re: Discussion of Options and Cost Estimates for Projected New Sewer Lagoon

Mr. Edwards and Mr. Conklin explained that this is a project of critical importance due to the present Park County sewer lagoon approaching the end of its useful life. Erik Wachob from Engineering Associates presented different options and cost estimates ranging from \$2.7mm to \$4.3mm to either upgrade the present county lagoon or partner with the City of Cody to improve their ability to accept the rural septage from local haulers. Cody Public Works Director Phillip Bowman expressed concerns with both the quantity and quality of the septage that could be brought in and noted there will need to be improved monitoring to see that heavy contaminants are not dumped. The Commissioners thanked the presenters and directed Mr. Edwards to continue working toward a viable solution with the City of Cody.

Park County Engineer, Brian Edwards and Erik Wachob of Engineering Associates Re: Presentation of Draft Preliminary Report – TAP Project-- Powell Area Pathways Planning

Mr. Edwards and Mr. Wachob reported that a community survey regarding pedestrian usage of county roads just north of Powell yielded 300 responses. They noted that this is a remarkable rate of return and that citizens responding have a very high level

of concern for walkers, bikers and joggers on these roads because there is little to no shoulder for them to stay out of the lane of vehicular travel.

The County is proposing to apply for a Transportation Alternatives Program (TAP) grant to help fund a project to build new pathways and provide the necessary separation between motorist and pedestrians along the roads identified in the draft report. The maximum award amount is up to \$500,000 and there is a cost share requirement of 80/20. The county will have to provide a statement of intent to apply by April 15, 2022 with the application due July 15, 2022. If awarded, design and permitting could start in 2023 and construction in 2024. The Commissioners thanked the presenters and asked them to continue on with the proposed project.

Executive Session: pursuant to W.S. § 16-4-405 (ix)

Commissioner Thiel made a motion to enter into executive session. Commissioner Mangold seconded and the motion carried.

Commissioner Overfield made a motion to exit from executive session. Commissioner Tilden seconded and the motion carried. No decisions were made in or as a result of this executive session.

Discussion of Option for Exemptions on the New Mineral Bill, Treasurer Barb Poley, Assessor Pat Meyer and County Attorney Bryan Skoric

Treasurer Poley discussed the Mineral Bill that was passed by the Legislature in 2020 and will be going into effect on January 1, 2022. The bill addresses the payment of ad valorem taxes on mineral production and will change the billing process currently in place as well as how and where the mineral companies will report monthly production to the State of Wyoming. Treasurer Poley asked the Commissioners if they wanted to exempt companies that have requested to stay on the current payment plan and if so to make a motion to reflect that decision. The County has had two companies of seventeen that are eligible, request an exemption. Commissioner Thiel questioned how much of a burden this is on the companies and remarked that he doesn't see any advantage to the company as they will still have to pay the taxes over time. Attorney Skoric doesn't see any advantage to the program either.

Commissioner Overfield made a motion to deny exemptions as presented by Treasurer Poley. Commissioner Thiel seconded and the motion carried.

Park County Commissioners Re: Continued Discussion of Buck Creek Estates Major Subdivision Application

Chairman Livingston provided the background and recent history of the Buck Creek Estates Major Subdivision application process. The Chairman stated that Developer, Brian Shumard had been asked to complete and provide results of a hydrological study of the proposed development area to prove that there would be sufficient ground water to supply all of the proposed lots. Mr. Shumard drilled two wells and provided that well data to the County Planning Department and the data was sent on to Karl Toboga at the office of Wyoming State Geological Survey for analysis. Mr. Taboga stated in an email to Planning Director Hill that the information provided by Mr. Shumard was not sufficient to determine if the Willwood aquifer could provide sufficient groundwater to the subdivision while being protective of wells on adjacent properties. Mr. Shumard's attorney Joey Darah addressed the Commissioners and stated that his client has met all the standards as written in the Park County Development Regulations and that the Commissioners should grant approval of Mr. Shumard's application today. The Chairman noted again that the water data is not sufficient and if a vote was taken today and it failed Mr. Shumard would have to wait one entire year to resubmit his application and it would be in his best interest to continue to work through the issues. Mr. Darah and Mr. Shumard asked the Commissioners to be very specific about what standards and guidelines need to be met to obtain approval for the Buck Creek Estates application. All agreed to work together to provide the needed information. No decision was made and discussion will continue as further information is provided.

Adjourn

Commissioner Overfield made a motion to adjourn the meeting. Commissioner Tilden seconded and the motion carried.

Lee Livingston, Chairman

Dossie Overfield, Vice Chairman

Scott Mangold, Commissioner

Lloyd Thiel, Commissioner

Joe Tilden, Commissioner

Attest:

Colleen Renner, County Clerk



PARK COUNTY - NORTH POWELL BICYCLE & PEDESTRIAN FACILITIES PLAN
Report Review Meeting with Stakeholders
11:00 am November 10, 2021

1. Re-introduction of stakeholders:
 - a. WYDOT
 - b. Park County Engineers
 - c. City of Powell
 - d. Northwest College
 - e. Park County School District #1
 - f. Shoshone Irrigation District

2. Presentation of report
 - a. Lack of separation between trucks / passenger vehicles and pedestrians / bicyclists
 - b. Study area
 - c. Field reconnaissance
 - d. Existing pathway facilities
 - e. Community survey
 - f. Potential pathway routes
 - g. Conceptual cost estimates

3. TAP funding

4. Input regarding location alternatives

5. Input regarding funding arrangements

Draft Report Review MEETING
 SIGN IN SHEET - PLEASE PRINT

NAME	COMPANY	PHONE NUMBER	EMAIL
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5. Tom Churchil	Churchill Farms	899-4106	
6. Holly Schacter	Northwest College	202-0196	colby.schacter@nw.edu
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9. Taryn Demars	WYDOT		taryn.demars1@wyo.gov
10. David Herdt	WYDOT	307-777-4862	
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12. Ben McDemmo	PARK COUNTY	307-527-8527	b.mcdemold@parkcounty.us
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14. Rebekah Burns	Park County Member/Board	307-754-3494	rebekah.burns@parkcounty.wy
15. SUE MANDOLD	PARK CO.	202 0527	SMANDOLD@parkcounty.us
16. Andy Metzler	City of Powell	202-0409	ametzel@cityofpowell.com
17.			